

Highway Cabinet Member Decision Session

Thursday 8 January 2015 at 10.00 am

To be held at the Town Hall,
Pinstone Street, Sheffield, S1 2HH

The Press and Public are Welcome to Attend

Members of the public can attend the sessions to make representations to the Cabinet Member.

If you wish to speak you will need to register by contacting Democratic Services (contact details overleaf) **no later than 10.00 am** on the last working day before the meeting.

PUBLIC ACCESS TO THE MEETING

Executive decisions in relation to Highway matters will be taken at Highway Cabinet Member Decisions Sessions. The Cabinet Member for Business, Skills and Development, Councillor Leigh Bramall, will be present at the sessions to hear any representations from members of the public and to approve Executive Decisions.

Should there be substantial public interest in any of the items the Cabinet Member may wish to call a meeting of the Cabinet Highways Committee

A copy of the agenda and reports is available on the Council's website at www.sheffield.gov.uk. You can also see the reports to be discussed at the meeting if you call at the First Point Reception, Town Hall, Pinstone Street entrance. The Reception is open between 9.00 am and 5.00 pm, Monday to Thursday and between 9.00 am and 4.45 pm. on Friday. You may not be allowed to see some reports because they contain confidential information. These items are usually marked * on the agenda.

Members of the public can attend the sessions to make representations to the Cabinet Member. If you wish to speak you will need to register by contacting Simon Hughes **no later than 10.00 am on the last working day before the meeting** via email at simon.hughes@sheffield.gov.uk or phone 0114 273 4014

Recording is allowed at Highway Cabinet Member Decisions Sessions under the direction of the Cabinet Member. Please see the website or contact Democratic Services for details of the Council's protocol on audio/visual recording and photography at council meetings.

If you would like to attend the meeting please report to the First Point Reception desk where you will be directed to the meeting room. Meetings are normally open to the public but sometimes the Cabinet Member may have to consider an item in private. If this happens, you will be asked to leave. Any private items are normally left until last.

The Cabinet Member's decisions are effective six working days after the meeting has taken place, unless called-in for scrutiny by the relevant Scrutiny Committee or referred to the City Council meeting, in which case the matter is normally resolved within the monthly cycle of meetings.

If you require any further information please contact Simon Hughes on 0114 273 4014 or email simon.hughes@sheffield.gov.uk.

FACILITIES

There are public toilets available, with wheelchair access, on the ground floor of the Town Hall. Induction loop facilities are available in meeting rooms.

Access for people with mobility difficulties can be obtained through the ramp on the side to the main Town Hall entrance.

**HIGHWAY CABINET MEMBER DECISION SESSION
8 JANUARY 2015**

Agenda

- 1. Exclusion of Press and Public**
To identify items where resolutions may be moved to exclude the press and public
- 2. Declarations of Interest** (Pages 1 - 4)
Members to declare any interests they have in the business to be considered at the meeting
- 3. Minutes of Previous Session** (Pages 5 - 10)
Minutes of the Session held on 13 November 2014
- 4. Public Questions and Petitions** (Pages 11 - 14)
 - (a) New Petitions
To record the receipt of petitions (a) containing 40 signatures requesting the removal of the central grass verge on Butchill Avenue and (b) containing 12 signatures requesting improved road safety measures on Sharrow Vale Road
 - (b) Outstanding Petitions
Report of the Executive Director, Place
- 5. 20mph Programme for 2015/16 and a Review of the Boundaries of the Areas** (Pages 15 - 36)
Report of the Executive Director, Place
- 6. Deep Lane Cycle Crossing Consultation** (Pages 37 - 46)
Report of the Executive Director, Place
- 7. Objections to Proposed 20mph Speed Limit in Hackenthorpe and the Area around London Road** (Pages 47 - 62)
Report of the Executive Director, Place
- 8. Coisley Hill - Objections to Proposed Traffic Calming** (Pages 63 - 82)
Report of the Executive Director, Place
- 9. Cat Lane/Carfield Lane - Proposed Prohibition of Driving Order** (Pages 83 - 94)
Report of the Executive Director, Place

NOTE: The next Highway Cabinet Member Decision Session will be held on Thursday 12 February 2015 at 10.00 am

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ADVICE TO MEMBERS ON DECLARING INTERESTS AT MEETINGS

If you are present at a meeting of the Council, of its executive or any committee of the executive, or of any committee, sub-committee, joint committee, or joint sub-committee of the authority, and you have a **Disclosable Pecuniary Interest (DPI)** relating to any business that will be considered at the meeting, you must not:

- participate in any discussion of the business at the meeting, or if you become aware of your Disclosable Pecuniary Interest during the meeting, participate further in any discussion of the business, or
- participate in any vote or further vote taken on the matter at the meeting.

These prohibitions apply to any form of participation, including speaking as a member of the public.

You **must**:

- leave the room (in accordance with the Members' Code of Conduct)
- make a verbal declaration of the existence and nature of any DPI at any meeting at which you are present at which an item of business which affects or relates to the subject matter of that interest is under consideration, at or before the consideration of the item of business or as soon as the interest becomes apparent.
- declare it to the meeting and notify the Council's Monitoring Officer within 28 days, if the DPI is not already registered.

If you have any of the following pecuniary interests, they are your **disclosable pecuniary interests** under the new national rules. You have a pecuniary interest if you, or your spouse or civil partner, have a pecuniary interest.

- Any employment, office, trade, profession or vocation carried on for profit or gain, which you, or your spouse or civil partner undertakes.
- Any payment or provision of any other financial benefit (other than from your council or authority) made or provided within the relevant period* in respect of any expenses incurred by you in carrying out duties as a member, or towards your election expenses. This includes any payment or financial benefit from a trade union within the meaning of the Trade Union and Labour Relations (Consolidation) Act 1992.

*The relevant period is the 12 months ending on the day when you tell the Monitoring Officer about your disclosable pecuniary interests.

- Any contract which is made between you, or your spouse or your civil partner (or a body in which you, or your spouse or your civil partner, has a beneficial interest) and your council or authority –
 - under which goods or services are to be provided or works are to be executed; and
 - which has not been fully discharged.

- Any beneficial interest in land which you, or your spouse or your civil partner, have and which is within the area of your council or authority.
- Any licence (alone or jointly with others) which you, or your spouse or your civil partner, holds to occupy land in the area of your council or authority for a month or longer.
- Any tenancy where (to your knowledge) –
 - the landlord is your council or authority; and
 - the tenant is a body in which you, or your spouse or your civil partner, has a beneficial interest.
- Any beneficial interest which you, or your spouse or your civil partner has in securities of a body where -
 - (a) that body (to your knowledge) has a place of business or land in the area of your council or authority; and
 - (b) either -
 - the total nominal value of the securities exceeds £25,000 or one hundredth of the total issued share capital of that body; or
 - if the share capital of that body is of more than one class, the total nominal value of the shares of any one class in which you, or your spouse or your civil partner, has a beneficial interest exceeds one hundredth of the total issued share capital of that class.

If you attend a meeting at which any item of business is to be considered and you are aware that you have a **personal interest** in the matter which does not amount to a DPI, you must make verbal declaration of the existence and nature of that interest at or before the consideration of the item of business or as soon as the interest becomes apparent. You should leave the room if your continued presence is incompatible with the 7 Principles of Public Life (selflessness; integrity; objectivity; accountability; openness; honesty; and leadership).

You have a personal interest where –

- a decision in relation to that business might reasonably be regarded as affecting the well-being or financial standing (including interests in land and easements over land) of you or a member of your family or a person or an organisation with whom you have a close association to a greater extent than it would affect the majority of the Council Tax payers, ratepayers or inhabitants of the ward or electoral area for which you have been elected or otherwise of the Authority's administrative area, or
- it relates to or is likely to affect any of the interests that are defined as DPIs but are in respect of a member of your family (other than a partner) or a person with whom you have a close association.

Guidance on declarations of interest, incorporating regulations published by the Government in relation to Disclosable Pecuniary Interests, has been circulated to you previously.

You should identify any potential interest you may have relating to business to be considered at the meeting. This will help you and anyone that you ask for advice to fully consider all the circumstances before deciding what action you should take.

In certain circumstances the Council may grant a **dispensation** to permit a Member to take part in the business of the Authority even if the member has a Disclosable Pecuniary Interest relating to that business.

To obtain a dispensation, you must write to the Monitoring Officer at least 48 hours before the meeting in question, explaining why a dispensation is sought and desirable, and specifying the period of time for which it is sought. The Monitoring Officer may consult with the Independent Person or the Council's Standards Committee in relation to a request for dispensation.

Further advice can be obtained from Gillian Duckworth, Interim Director of Legal and Governance on 0114 2734018 or email gillian.duckworth@sheffield.gov.uk.

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Highway Cabinet Member Decision Session

Highway Cabinet Member Decision Session held 13 November 2014

PRESENT: Councillor Leigh Bramall (Cabinet Member for Business, Skills and Development)

ALSO IN ATTENDANCE: Dick Proctor (Transport Planning Manager)
Moaz Khan (Interim Head of Transport, Traffic and Parking Services)
Nat Porter (Highways Officer)
Simon Nelson (Traffic Management Engineer)
Mark Simons (Highways Officer)

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1. EXCLUSION OF PRESS AND PUBLIC

1.1 No items were identified where it was proposed to exclude the public and press.

2. DECLARATIONS OF INTEREST

2.1 There were no declarations of interest.

3. MINUTES OF PREVIOUS SESSION

3.1 The minutes of the previous Session held on 9 October 2014 were approved as a correct record.

4. PUBLIC QUESTIONS AND PETITIONS

4.1 New Petitions

There were no new petitions to report.

4.2 Public Question in respect of the Wadsley Bridge Supermarket Highway Works

Matt Turner commented that, on 9 May 2013, this Session was told that the zebra crossings would not be removed as part of the Wadsley Bridge Supermarket, as recorded in the minutes at paragraph 3.7, in response to his concern about them being removed. They had now been removed and the replacement crossings were not functioning. There were now no crossings across these roads (Penistone Road and Leppings Lane). He therefore asked why had these been removed, and why were they removed before the replacement crossings were finished?

Mark Simons, Highways Officer, commented that he shared Mr Turner's frustrations. He had not been in attendance at the Session on 9 May 2013 so couldn't comment on what had been said but it was always the intention to remove the crossing. The current situation was, however, unacceptable.

The highway scheme for Sainsbury's had proved problematic as he believed

Sainsburys had not done enough work to establish the locations of the Statutory Undertakers equipment. This has resulted in some necessary changes to the design as the scheme was being constructed (and as a result some delay had occurred). Slightly further along the A61 was the Council's Pinchpoint scheme. The work here was being undertaken by Amey and managed by the Council. The Pinchpoint Scheme was planned well in advance and the Statutory Undertakers work programmed accordingly.

Mark Simons managed what were known as Section 278 schemes where developers were allowed to appoint their own contactors to undertake works on the public highway. Whilst the Council could no longer insist on undertaking highway works for private developers it still had a responsibility to ensure that works were undertaken in the right way.

The Sainsbury's scheme had had design changes and issues with the statutory undertakers works causing delay and this has resulted in the scheme evolving whilst on site. As a result of the issues arising from this, Mr Simons had suggested that the Council reviewed how all Section 278 legal agreements were managed in the future. At the zebra crossing referred to by Mr Turner, Mr Simons had been promised that alternative arrangements would be operational within two days which had not been the case.

There had been issues with the road surface where the zebra crossing was and as a result this had to be resurfaced. A Road Safety Audit had been undertaken which accepted that for a short period of time, pedestrians could be signed to cross in the location of the old zebra crossing, but to take care.

The Council had not received a satisfactory response from Siemens, who were responsible for providing the permanent alternative crossing facilities, despite numerous requests.

RESOLVED: That the Director of Regeneration and Development Services be requested to liaise with Sainsbury's and inform them of the City Council's request that, should a permanent crossing not be installed to replace the zebra crossing which had been removed as part of the works associated with the Wadsley Bridge Supermarket, a temporary crossing be installed as soon as possible in the interests of road safety.

5. PETITION IN RESPECT OF BANNER CROSS/ECCLESALL ROAD PROPOSED PARKING METER SCHEME

- 4.1 The Executive Director, Place submitted a report providing an update on investigations subsequent to the decision of 12th June 2014 regarding a petition received considering the proposed pay and display parking scheme on Ecclesall Road at Banner Cross district centre, and seeking a decision on the petition and the scheme.
- 4.2 Viv Lockwood, Secretary of the Banner Cross Neighbourhood Group, attended the Session to make representations to the Cabinet Member. He commented that the Group had taken a neutral position on the proposals from the beginning as they

- were aware that they were contentious.
- 4.3 At the last Session a local resident had stated that the majority of traders were not in favour of the proposals. This was not true and the Group had sought comment from both traders and local residents.
- 4.4 The economy of Banner Cross was fragile and a number of shops had recently closed or were in danger of having to close. The area had seen a drift of customer footfall down the hill from up the hill. Mr Lockwood had visited local traders to ask if they were in favour of the proposals. A number had commented that they were overwhelmingly in favour. This was because they had been losing trade as a result of vehicle movements. After 9:30 a.m. when waiting restrictions had finished a number of people were bringing their cars down and parking in front of the shops resulting in a loss of trade as potential customers often went elsewhere if they saw that they couldn't park near the shop.
- 4.5 Some of the comments from traders was that they hoped that the bus lane restrictions could operate in a different way. However, they hoped that something could be done to resolve the problem. Those traders who had supported the proposals often had their own car parks so the issues were not as apparent.
- 4.6 Mr Lockwood was concerned by the steady decline in the economy of Banner Cross and he was frightened that the area would be left with a dead economy. It would only take another three or four shops to close down for it to be the final straw.
- 4.7 In respect of the residents' petition, the Banner Cross Neighbourhood Group had 270 members and it was not Mr Lockwood's experience that there was a large body of resident opinion against the proposals. He believed the scheme addressed many of their concerns. When a number of residents had been told of the possibility of installing a parking meter in the area their instinctive reaction was to oppose it. However, when the reasons for its introduction were clearly explained to them they often changed their view.
- 4.8 In conclusion, Mr Lockwood commented that he hoped the Cabinet Member could support the original proposal and requested that this be approved on an experimental basis to assess its impact.
- 4.9 Councillor Leigh Bramall, Cabinet Member for Business, Skills and Development, commented that this was a very difficult issue. He understood the views of residents that they wanted to park near their homes but also that a thriving district centre was vital and this presented a conflict which was difficult to resolve.
- 4.10 Councillor Bramall commented that he believed more work should be done on this. There was currently an issue of funding. He was minded to request more work be undertaken on investigating journey patterns and the turn over of traders. He requested that this be undertaken and a further report be submitted to this Session in the new financial year.
- 4.11 **RESOLVED:** That a decision on the scheme be deferred pending further work

investigating journey patterns in the area and the impact on local trade and a further report be submitted to a future Session in the new financial year.

4.12 Reasons for Decision

4.12. There was a difficult balance in the area of the needs of traders for customers to
1 park outside their shops and local residents who wished to park their cars outside their house. Further work needed to be undertaken to assess both sides needs and the impact on local trade and parking in the area before a decision could be taken.

4.13 Alternatives Considered and Rejected

4.13. To approve the recommendation not to implement a scheme in the area. Local
1 traders had emphasised the importance of a scheme being introduced to ensure a vibrant, thriving shopping area in Banner Cross. Further work needed to be undertaken to assess their needs as well as the wish of local residents to park their cars near their houses.

6. PENISTONE ROAD PROPOSED PROHIBITION OF LEFT TURN INTO HERRIES ROAD SOUTH

5.2 **RESOLVED:** That the item be withdrawn from consideration at the meeting pending further discussions with cycle interest groups.

7. OBJECTIONS TO PROPOSED 20MPH SPEED LIMIT IN DARNALL AND SHIRECLIFFE

6.1 The Executive Director, Place submitted a report describing the response from residents to the proposal to introduce a 20mph speed limit in Darnall and Shirecliffe, reports the receipt of objections and set out the Council's response.

6.2 **RESOLVED:** That:-

- (a) the Darnall and Shirecliffe 20mph Speed Limit Orders be made in accordance with the Road Traffic Regulation Act 1984;
- (b) the objectors be informed accordingly; and
- (c) the proposed 20mph speed limits be introduced.

6.3 Reasons for Decision

6.3.1 Reducing the speed of traffic in residential areas will, in the long term, reduce the number and severity of accidents, reduce the fear of accidents, encourage sustainable modes of travel and contribute towards the creation of a more pleasant, cohesive environment.

6.3.2 Having considered the objections to the introduction of a 20mph speed limit in Darnall and Shirecliffe the officer view is that the reasons set out in the report for making the Speed Limit Order outweigh the objections. The introduction of a 20mph speed limit in the area would be in-keeping with the City's approved 20mph Speed Limit Strategy.

6.4 **Alternatives Considered and Rejected**

6.4.1 Consideration has been given to omitting Wilfrid Road from the Darnall 20mph limit area. There are relatively few residential properties fronting Wilfrid Road, however it bounds one side of Darnall Community Park and play area. Following the receipt of these comments the local Ward Members were asked for their opinion on the inclusion of Wilfrid Road, either in total or in part. Two of the three Ward Councillors responded and both specifically asked for the speed limit on Wilfrid Road to be reduced to 20mph as originally planned.

6.4.2 The objections relate to the principle of introducing sign-only 20mph speed limits into residential areas, and therefore the approved Sheffield 20mph Speed Limit Strategy. As such, no alternative options have been considered. Speeds will be monitored and the addition of further measures will be considered if appropriate, as outlined in paragraph 4.13 of the report.

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SHEFFIELD CITY COUNCIL Highway Cabinet Member Decision Session

Report of: EXECUTIVE DIRECTOR, PLACE

Date: 8 January 2015

Subject: OUTSTANDING PETITIONS LIST

Author of Report: Jane White 0114 2736135

Summary:

List of outstanding petitions received by Transport & Highways

Recommendations:

To Note

Background Papers: None

Category of Report: OPEN

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No.	No. of Sigs	Description Of The Petition	Reported To Meeting On	Responsibility	Outcome Of Investigation To Be Reported To	Comments
1	12,571	Petition Requesting Road Safety Measured on Normanton Hill	02.07.14	Transport Planning	ICMD	November Cabinet approved principle of scheme for construction next year
2	287	Petition Requesting a Zebra Crossing Outside Hucklow Primary School	02.07.14	Transport Planning	ICMD	Lead petitioner responded to and informed that request will be assessed as Enhancement in line with Streets Ahead Core Investment Period (CIP) (or after CIP if zone complete).
3	11	Petition Requesting Action Regarding Car Parking Problems on White Lane	14.07.14	Transport Planning	ICMD	Lead petitioner responded to. A scheme assessment will be carried so that this request could be implemented with the relevant Streets Ahead Zone. If it fails to score highly enough to action when compared to other requests, it will not be progressed and the petitioner will be informed accordingly.
4	78	Petition Requesting Resurfacing and Other Road Safety Measures on Mill Road, Ecclesfield	03.09.14	Transport Planning	ICMD	Lead petitioner to be responded to.
5	33	Petition Requesting the Council to Support World Car Free Day	03.09.14	Transport Planning	ICMD	Lead petitioner to be responded to. We have sent general supportive response
6.	40	Petition requesting the removal of the central grass verge on Butchill Avenue	08.01.15			

7.	12	Petition requesting improved Road Safety measures on Sharrow Vale Road	08.01.15			
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SHEFFIELD CITY COUNCIL Individual Cabinet Member Decision

Report of: Executive Director, Place

Date: 8th January 2015

Subject: Sheffield 20mph Speed Limit Strategy:
2015/16 proposed programme of 20mph areas and
boundary changes.

Author of Report: Susie Pryor

Summary:

This report describes the proposed programme of 20mph areas for 2015/16 and changes to the boundaries of some of these areas.

Reasons for Recommendations:

Reducing the speed of traffic in residential areas will, in the long term, reduce the number and severity of accidents, reduce the fear of accidents, encourage sustainable modes of travel and contribute towards the creation of a more pleasant, cohesive environment.

Recommendations:

1. Approve the 2015/16 programme of proposed 20mph areas described in paragraph 4.12 of this report.
2. Prioritise the introduction of future 20mph schemes by both their road injury collision record and the potential to co-ordinate their introduction with the Streets Ahead maintenance programme.
3. That each classified road within a proposed 20mph area be assessed for inclusion or exclusion on a case by case basis.
4. That the boundary review be continued for all the remaining potential 20mph areas in the City.

Background Papers: Appendix A: Plans of the proposed 20mph areas.

Category of Report: OPEN

Statutory and Council Policy Checklist

Financial Implications
YES Cleared by: Gaynor Saxton
Legal Implications
YES Cleared by: Nadine Winter
Equality of Opportunity Implications
YES Cleared by: Ian Oldershaw
Tackling Health Inequalities Implications
NO
Human rights Implications
NO:
Environmental and Sustainability implications
NO
Economic impact
NO
Community safety implications
NO
Human resources implications
NO
Property implications
NO
Area(s) affected
Citywide
Relevant Cabinet Portfolio Leader
Jayne Dunn
Relevant Scrutiny Committee if decision called in
Culture, Economy and Sustainability
Is the item a matter which is reserved for approval by the City Council?
NO
Press release
YES

SHEFFIELD 20MPH SPEED LIMIT STRATEGY: 2015/16 PROPOSED PROGRAMME OF 20MPH AREAS AND BOUNDARY CHANGES.

1.0 SUMMARY

1.1 This report describes the proposed programme of 20mph areas for 2015/16 and changes to the boundaries of some of these areas. It also includes recommendations for the prioritisation of 20mph schemes and the issue of whether to include classified roads.

2.0 WHAT DOES THIS MEAN FOR SHEFFIELD PEOPLE?

2.1 Reducing the average speed of drivers in residential areas is expected, over time, to bring about a reduction in the number and severity of traffic accidents, thus helping to create *safe and secure communities*. Implementing the 20mph speed limits described in this report together with an ongoing programme of publicity and driver education would contribute to the creation of a safer residential environment and a *Great Place to Live*. The response to the consultation contributes to the *working better together* value of the Council Plan *Standing up for Sheffield*.

3.0 OUTCOME AND SUSTAINABILITY

3.1 These schemes represent a step towards influencing driver behaviour and establishing 20mph as the default maximum appropriate speed in residential areas. This will contribute to the delivery of:

- the '*sustainable and safe transport*' objective of the Corporate Plan;
- Policy W of the Sheffield City Region Transport Strategy 2011-2026 (*To encourage safer road use and reduce casualties on our roads*);
- the Council's Vision For Excellent Transport In Sheffield (*a better environment; a healthier population; a safer Sheffield*); and
- the Fairness Commission's recommendation for a 20mph speed limit on all residential roads in Sheffield.

4.0 REPORT

Introduction

4.1 In February 2011, Full Council adopted the following motion: "*To bring forward plans for city-wide 20mph limits on residential roads (excluding main roads)*". This led to the adoption of the Sheffield 20mph Speed Limit Strategy by the Cabinet Highways Committee on 8th March 2012, the long-term aim of which is to establish 20mph as the maximum appropriate speed in residential

areas of Sheffield¹.

- 4.2 The first seven areas were introduced during the financial years 2013/14 and a further seven areas are being implemented in 2014/15. These 20mph speed limits are indicated by traffic signs and road markings only, that is, they do not include any additional 'physical' traffic calming measures such as road humps.
- 4.3 Prior to the adoption of the Sheffield 20mph Speed Limit Strategy, a further 24 large and 10 small traffic calmed 20mph zones were established, the smaller ones generally resulting from planning conditions placed on new housing developments. It is currently anticipated that a similar number of sign-only 20mph speed limits will have been implemented by the end of the 2015/16 financial year. Taken together this represents approximately a third of the residential streets in the city. Appendix A contains a plan which shows existing 20mph areas, areas suitable for future 20mph speed limits, the classified road network and school locations.

Criteria for roads to be included

- 4.4 The Council's policy for 20mph schemes, that was approved in March 2012, is that A and B classified roads, major bus routes, and roads with an existing speed limit of 40mph or more will not be made subject to a 20mph speed limit and that there will be a presumption against including C-class roads (generally local distributor roads) within new 20mph speed limit areas.
- 4.5 Following consultation during the delivery of the sign only 20mph schemes introduced so far, requests have been made for the inclusion of a number of classified roads which fall within the boundary of a 20mph area.
- 4.6 Speed limits should both reflect the character of the road to which they apply but be realistic. The relevant Department for Transport guidance notes the importance of existing speeds when designating new speed limits: "*If the mean speed is already at or below 24 mph on a road, introducing a 20 mph speed limit through signing alone is likely to lead to general compliance with the new speed limit.*" (DfT Circular 01/2013).
- 4.7 However, it goes on to describe a variety of competing factors and demands that a local authority should take into consideration when considering altering a speed limit, and advises that these may be weighted differently according to the particular location in question.
- 4.8 If the existing vehicle speeds on a particular road are far above the maximum recommended by the Department for Transport and in the absence of funding for complementary traffic calming measures, then a 20mph limit would be artificially low. This is likely to result in excessive abuse and bring 20mph limits in general into disrepute.
- 4.9 For future 20mph areas it is recommended that classified roads be primarily assessed using speed survey data. Residential roads on which average

¹ [Sheffield City Council - Meeting of Cabinet Highways Committee on Thursday 8 March 2012](#)

speeds are 24mph or below will automatically be considered suitable. Any roads with average speeds of between 24mph and 27mph (which is above the current DfT guidelines as stated in paragraph 4.6) will be considered for a 20mph speed limit on a case by case basis using current DfT guidelines. Average speeds of 27mph and over, will only be considered if capital funding can be identified for the appropriate traffic calming measures that would be needed to reduce vehicle speeds. It should be noted that funding for such works is extremely limited due to continued reductions in the funding allocated by central government for highway works.

- 4.10 A similar process is also under consideration as part of assessing the suitability of roads in the City Centre for a 20mph speed limit. This will be subject to a separate ICMD report in February 2015.

20mph Area Programme 2015/16

- 4.11 In February 2014 a provisional programme of 20mph areas for the 2015/16 year was approved by the Cabinet member for road safety. This has since been revised with a view to more quickly fulfilling the council policy of introducing a 20mph speed limit in all suitable residential areas.
- 4.12 It is recommended that the following eleven areas should be made subject to a 20mph speed limit in 2015/16 on the basis of sufficient capital funding being available. The proposed 20mph areas are listed in priority order and will be implemented in this order, based on coordination the Streets Ahead programme. There may be insufficient capital funding to build all eleven schemes this year. If this is the case then the remaining schemes will be carried forward to be built in the 2016/17 year. Appendix A contains plans showing the extents of these proposed 20mph areas.
1. Gleadless Valley
 2. Stannington
 3. Park Academy
 4. Woodhouse
 5. Greystones and Whirlow
 6. Firth Park
 7. Hillsborough
 8. Wincobank
 9. Meadowhead, Greenhill and Beauchief
 10. Sharrow Vale
 11. Hurlfield
- 4.13 Before a 20mph scheme is implemented all households within the prospective area will receive a consultation leaflet about the sign-only 20mph speed limits, with residents invited to comment on or object to the introduction of a 20mph Speed Limit Order. Objections would be reported to a future meeting of the Cabinet Highways Committee.

Prioritising Future 20mph Schemes

- 4.14 From 2015/16 the intention is to continue to co-ordinate, where possible, the delivery of the majority of 20mph schemes with the Streets Ahead maintenance programme, whilst also addressing the areas with the very worst road injury collision records.
- 4.15 Each of these areas has been ranked by the number of collisions which resulted in injury during a five year period from 2008 to 2012. The collision data will be updated in 2015 to set the programme for 2016/17 and beyond.
- 4.16 Current Council policy is that there is a preference for 20mph areas to include at least one school. The number of schools in each area is listed in the priority spreadsheet. It should be noted that where a 20mph does not include a school, there is likely to be one nearby and there will be children making the school journey within that area.
- 4.17 It is recommended that the boundaries of potential future 20mph speed limit schemes be reviewed and with a view to maximising the size of each area. This may shorten the timescale for the delivery of 20mph speed limits in all suitable residential areas citywide, subject to available funding.

Relevant Implications

- 4.18 The 20mph areas described in this report will be funded by an allocation from the 2015/16 Local Transport Plan programme of £400,000, a potential £200,000 contribution from a Public Health grant and £10,000 contribution from Sheffield Park Academy School towards a 20mph speed limit in the area surrounding the school. This is a total of £610,000.

The allocation for 2015/16 has not yet been confirmed and the implementation of the programme will be subject to funding being available.

The financial allocation will include an allowance for:

- a commuted sum to cover the cost of the future maintenance, payable to Amey under the terms of the Streets Ahead contract; and
 - publicity to promote the benefits of lower speed limits in residential areas
- 4.19 The Council has a statutory duty to promote road safety and to ensure that any measures it promotes and implements are reasonably safe for all users. In making decisions of this nature the Council must be satisfied that the measures are necessary to avoid danger to pedestrians and other road users or for preserving or improving the amenities of the area through which the road runs. Providing that the Council is so satisfied then it is acting lawfully and within its powers
- 4.20 An Equality Impact Assessment was conducted for this report and concluded that safer roads and reduced numbers of accidents involving traffic and

pedestrians would fundamentally be positive for all local people regardless of age, sex, race, faith, disability, sexuality, etc. However, the most vulnerable members of society (i.e. the young and elderly) would particularly benefit from this initiative. No negative equality impacts were identified.

5.0 ALTERNATIVE OPTIONS CONSIDERED

5.1 That speed limits in residential areas across the City remain the same. However, this would lead to the same level of road accidents and vehicle speeds in residential areas.

6.0 REASONS FOR RECOMMENDATIONS

6.1 Reducing the speed of traffic in residential areas will, in the long term, reduce the number and severity of accidents, reduce the fear of accidents, encourage sustainable modes of travel and contribute towards the creation of a more pleasant, cohesive environment.

6.2 The introduction of a 20mph speed limit in these areas would be in-keeping with the City's approved 20mph Speed Limit Strategy.

7.0 RECOMMENDATIONS

7.1 Approve the 2015/16 programme of proposed 20mph areas described in paragraph 4.12 of this report.

7.2 Prioritise the introduction of future 20mph schemes by both their road injury collision record and the potential to co-ordinate their introduction with the Streets Ahead maintenance programme.

7.3 That each classified road within a proposed 20mph area be assessed for inclusion or exclusion on a case by case basis.

7.4 That the boundary review be continued for all the remaining potential 20mph areas in the City.

Simon Green
Executive Director, Place

APPENDIX A

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Sheffield 20mph Speed Limit Strategy

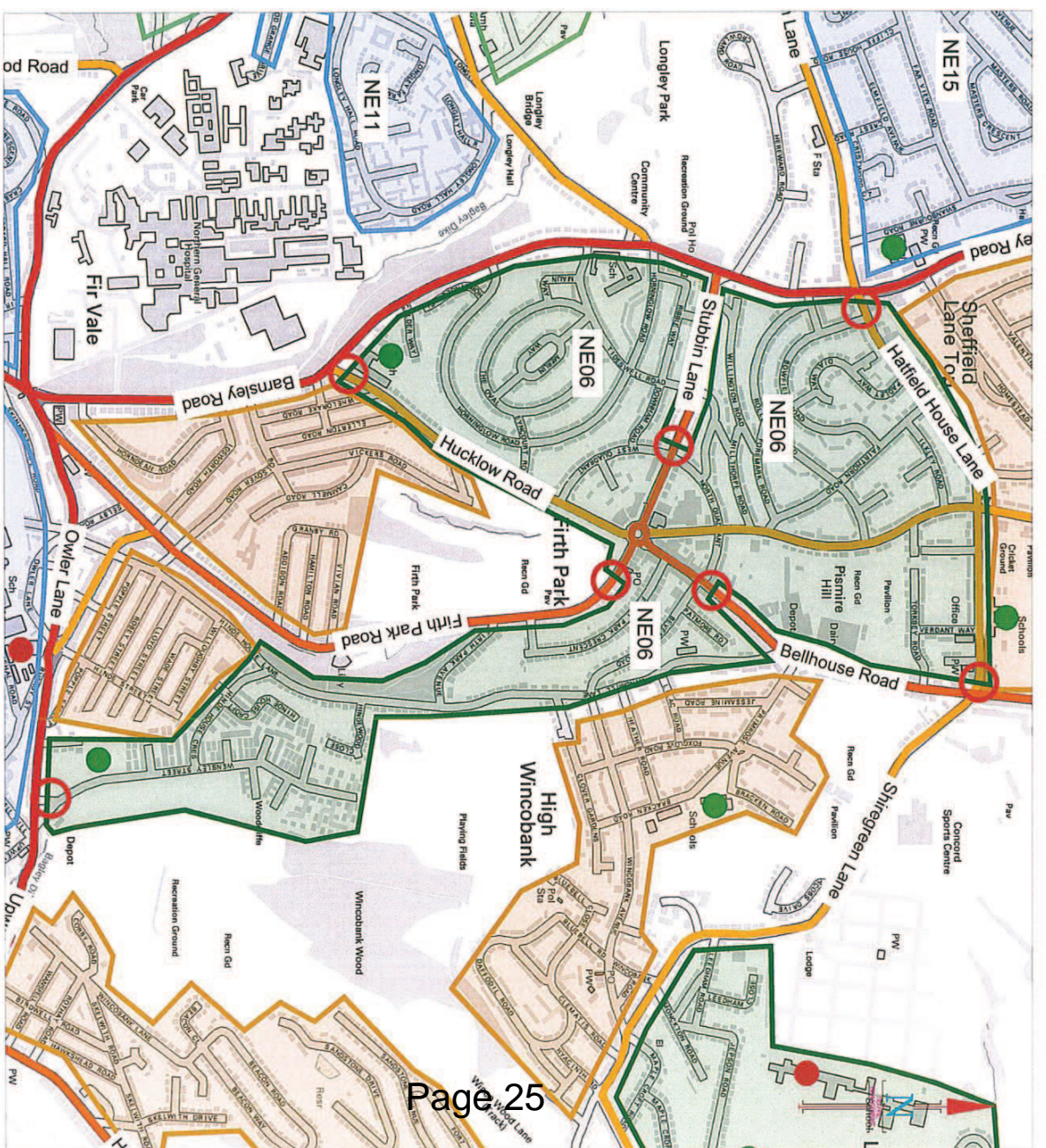
2015/16 Programme

Firth Park (NE06)

- Proposed 20mph area includes HBH Academy, Owlter Brook Nursery and Infant School and Hucklow Primary School as would be the Firth Park shopping area
- The 20mph limit would tie into existing Shiregreen, Wincobank Avenue area and Willoughby Road area 20mph Zones
- Speed data will be collected to confirm whether roads are suitable to be made subject to a 20mph speed limit

Key

- 'A' road
- 'B' road
- 'C' road
- Primary, Junior and Infant schools
- Secondary schools
- Potential future 20mph speed limit
- Existing 20mph zone
- Existing 20mph speed limit
- 2015-16 20mph speed limit programme
- Entry point with half-width red surfacing



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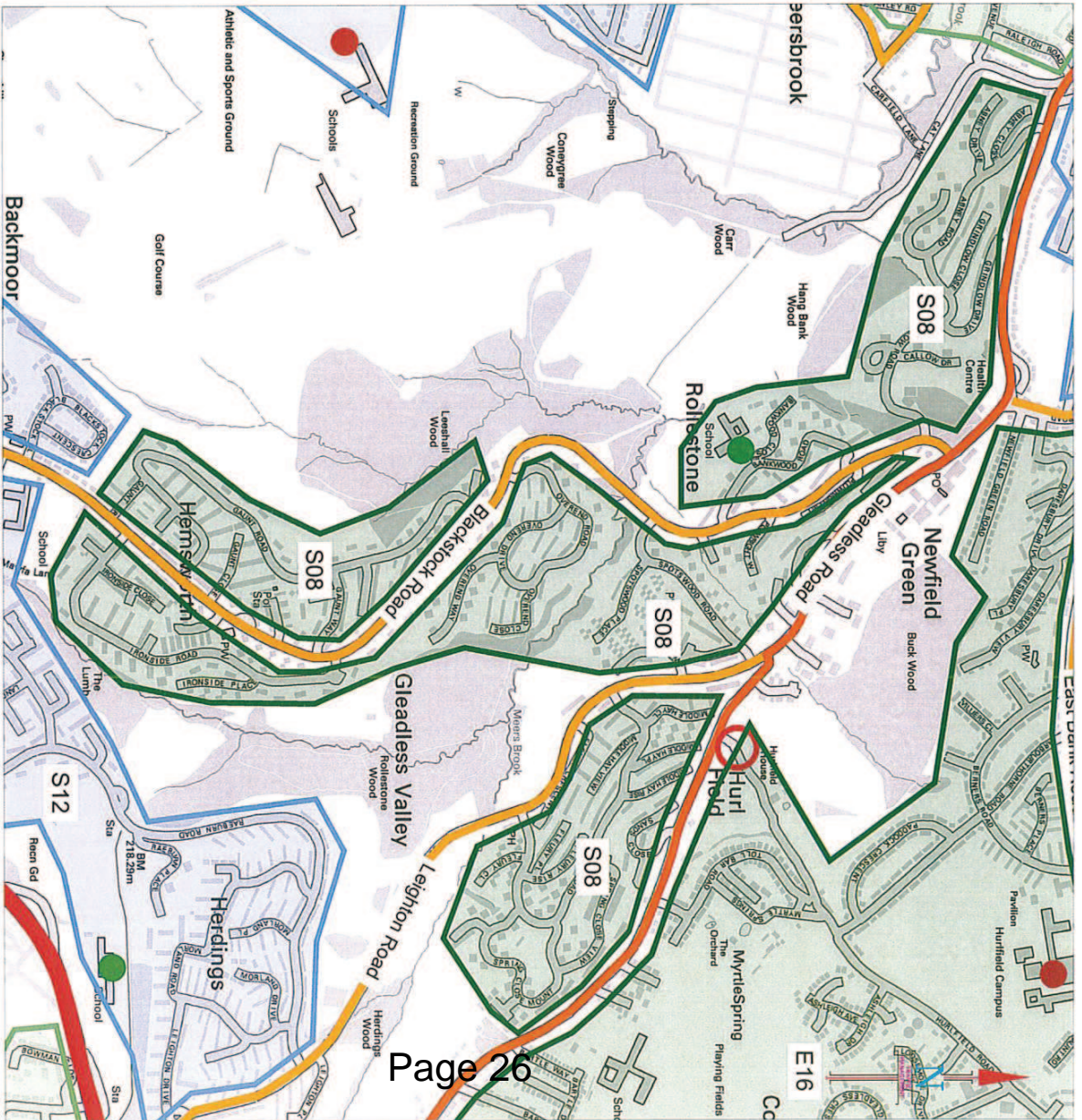
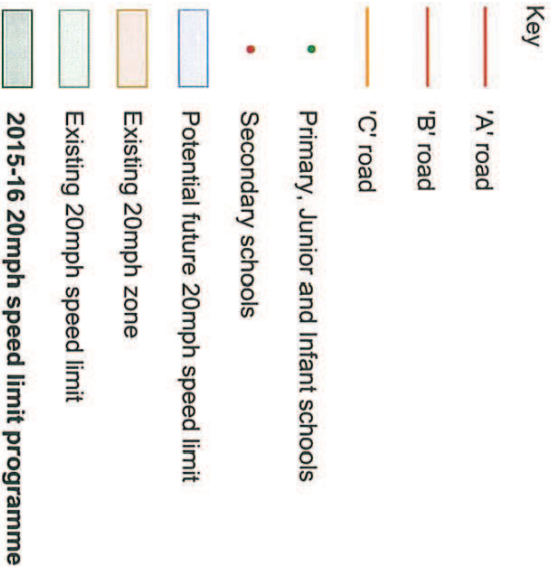


Sheffield 20mph Speed Limit Strategy

2015/16 Programme

Gleadless Valley (SE08)

- Proposed 20mph area includes Bankwood Primary School
- The existing 30mph limit would remain on Blackstock Road and Leighton Road
- Speed data will be collected to confirm whether roads are suitable to be made subject to a 20mph speed limit



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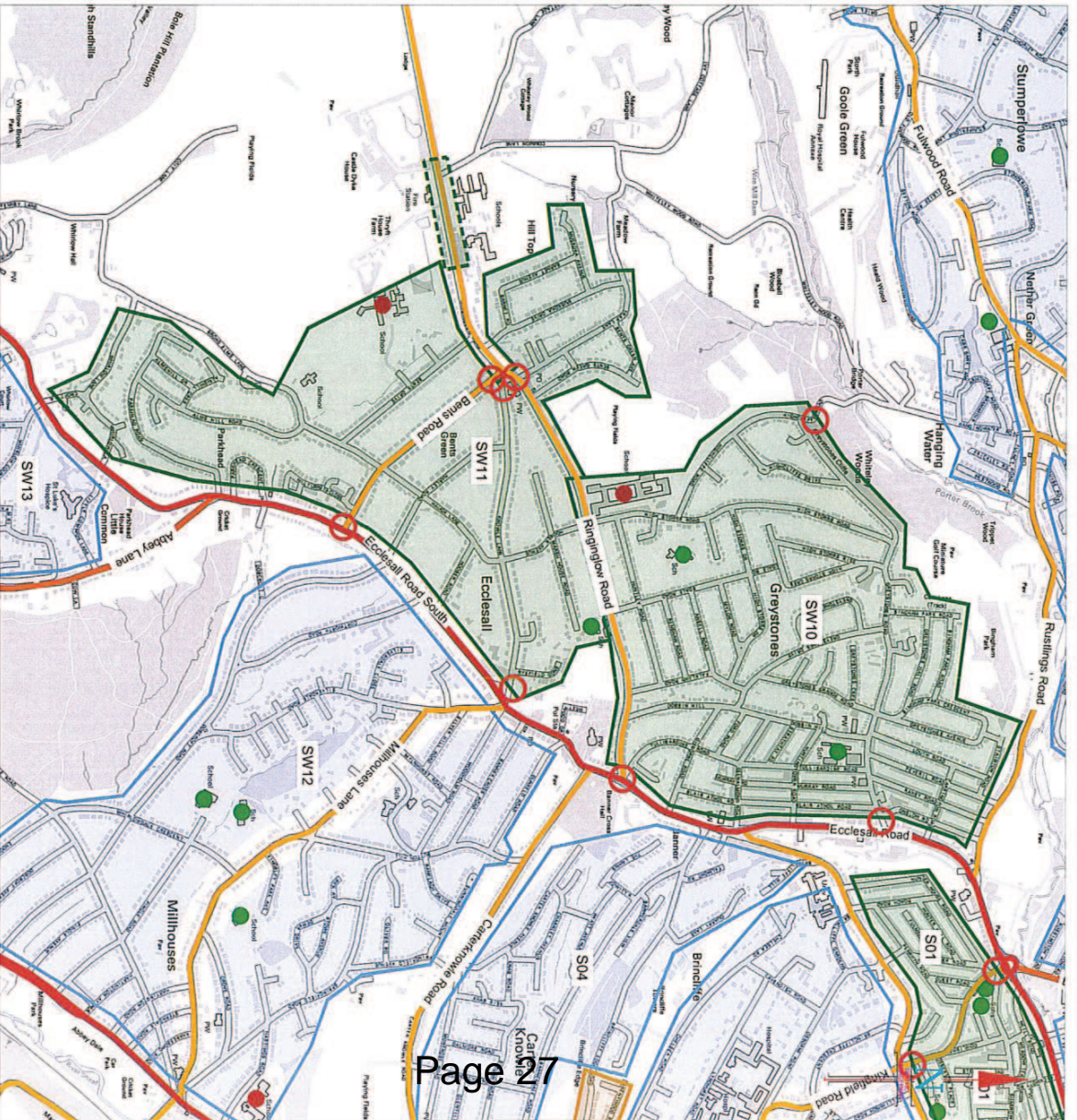
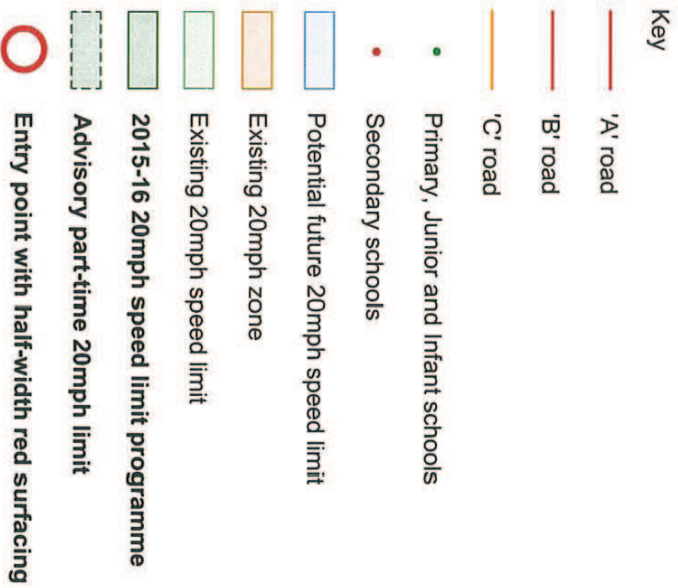
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Sheffield 20mph Speed Limit Strategy 2015/16 Programme

Greystones & Whirlow (SW10 & SW11)

- Proposed 20mph area includes High Storrs and Silverdale secondary schools, Ecclesall Church of England Junior School, Ecclesall Infant School and Greystones Primary School
- An advisory part-time 20mph limit would be introduced on Ringinglow Road around the entrance to Bents Green School, to operate at the start and end of the school day
- Speed data will be collected to confirm whether roads are suitable to be made subject to a 20mph speed limit



Sheffield 20mph Speed Limit Strategy

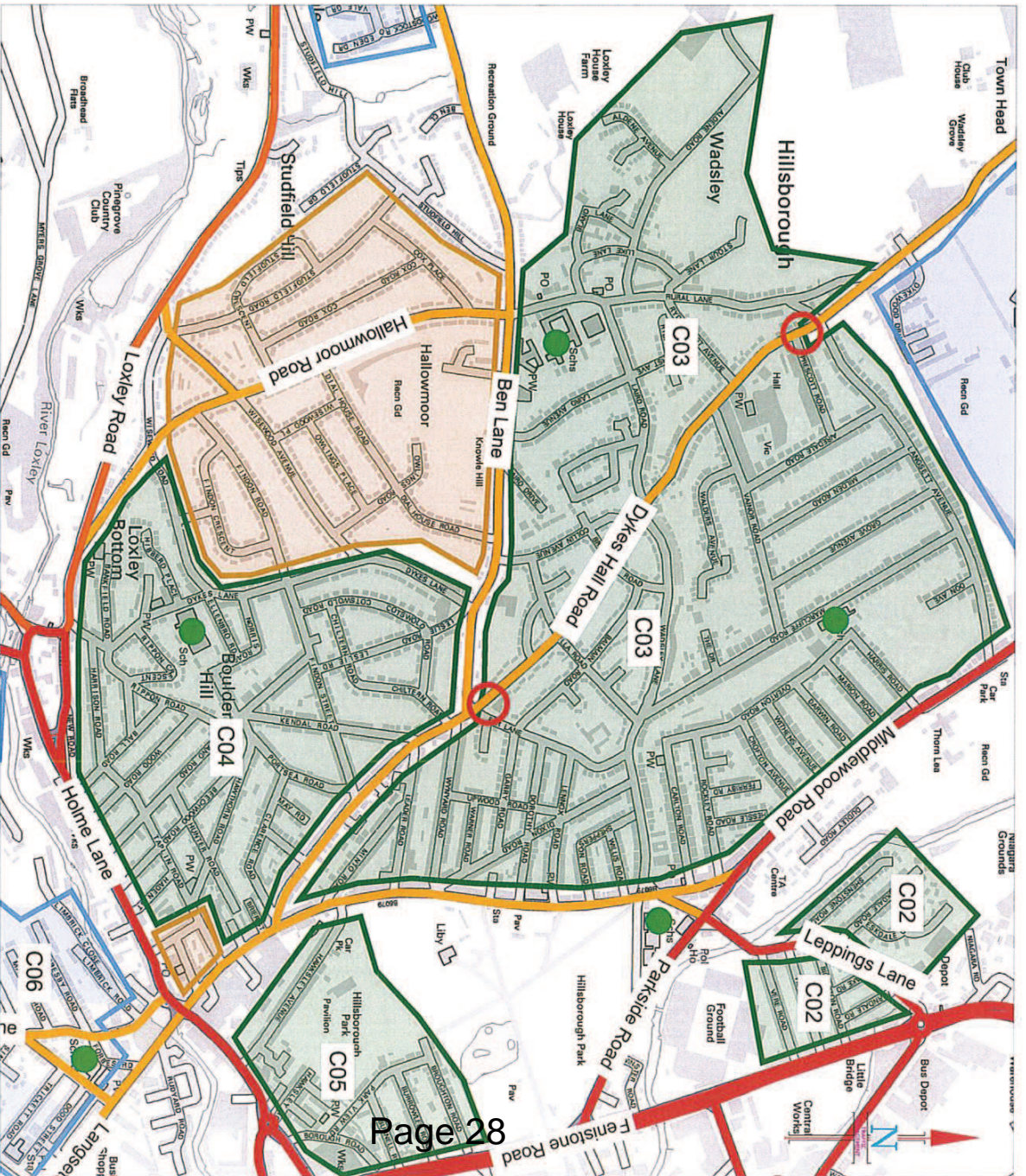
2015/16 Programme

Hillsborough (C02, C03, C04 & C05)

- Proposed 20mph area includes Marcliffe Community Primary School, Wisewood Community Primary School and Main Bridge Primary School
- The 20mph limit would tie into existing 20mph zones
- The existing 30mph limit would remain on Ben Lane, Middlewood Road, Leppings Lane and the southeastern part of Dykes Hall Road.
- The northwestern, traffic calmed part of Dykes Hall Road would be part of the 20mph area
- Speed data will be collected to confirm whether roads are suitable to be made subject to a 20mph speed limit

Key

- 'A' road
- 'B' road
- 'C' road
- Primary, Junior and Infant schools
- Secondary schools
- Potential future 20mph speed limit
- Existing 20mph zone
- 2015-16 20mph speed limit programme
- Entry point with half-width red surfacing



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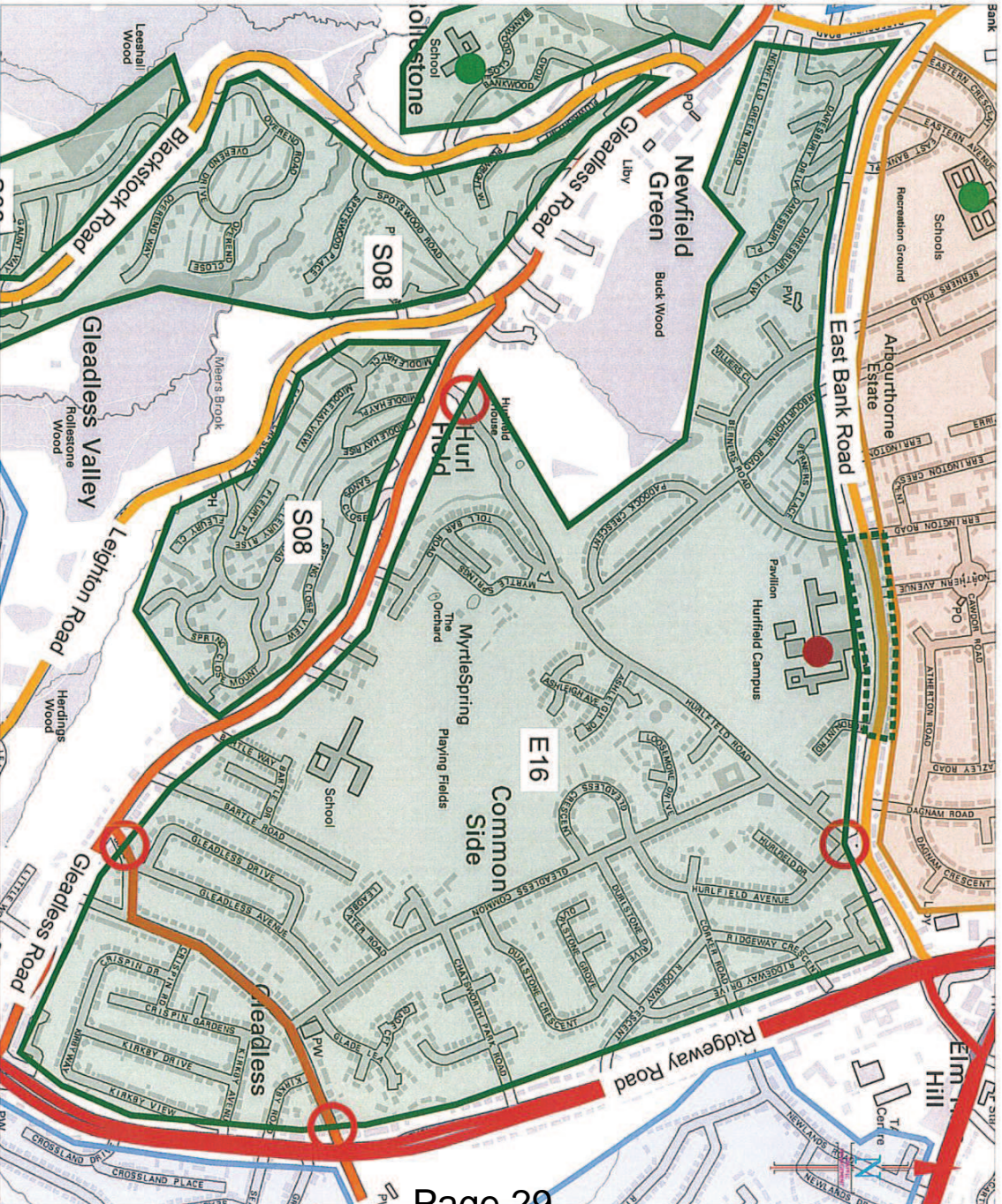
Sheffield 20mph Speed Limit Strategy 2015/16 Programme

Hurlfield (E16)

- Proposed 20mph area to be bounded by, but not include, East Bank Road, Ridgeway Road and Gleadless Road
- An advisory part-time 20mph speed limit would be introduced around the East Bank Road entrance to Sheffield Springs Academy to operate at the start and end of the school day
- Part of the B6063 Hollinsend Road would be included in the area
- Speed data will be collected to confirm whether roads are suitable to be made subject to a 20mph speed limit

Key

- 'A' road
- 'B' road
- 'C' road
- Primary, Junior and Infant schools
- Secondary schools
- Potential future 20mph speed limit
- Existing 20mph zone
- Existing 20mph speed limit
- 2015-16 20mph speed limit programme
- Advisory part-time 20mph speed limit
- Entry point with half-width red surfacing



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Sheffield 20mph Speed Limit Strategy

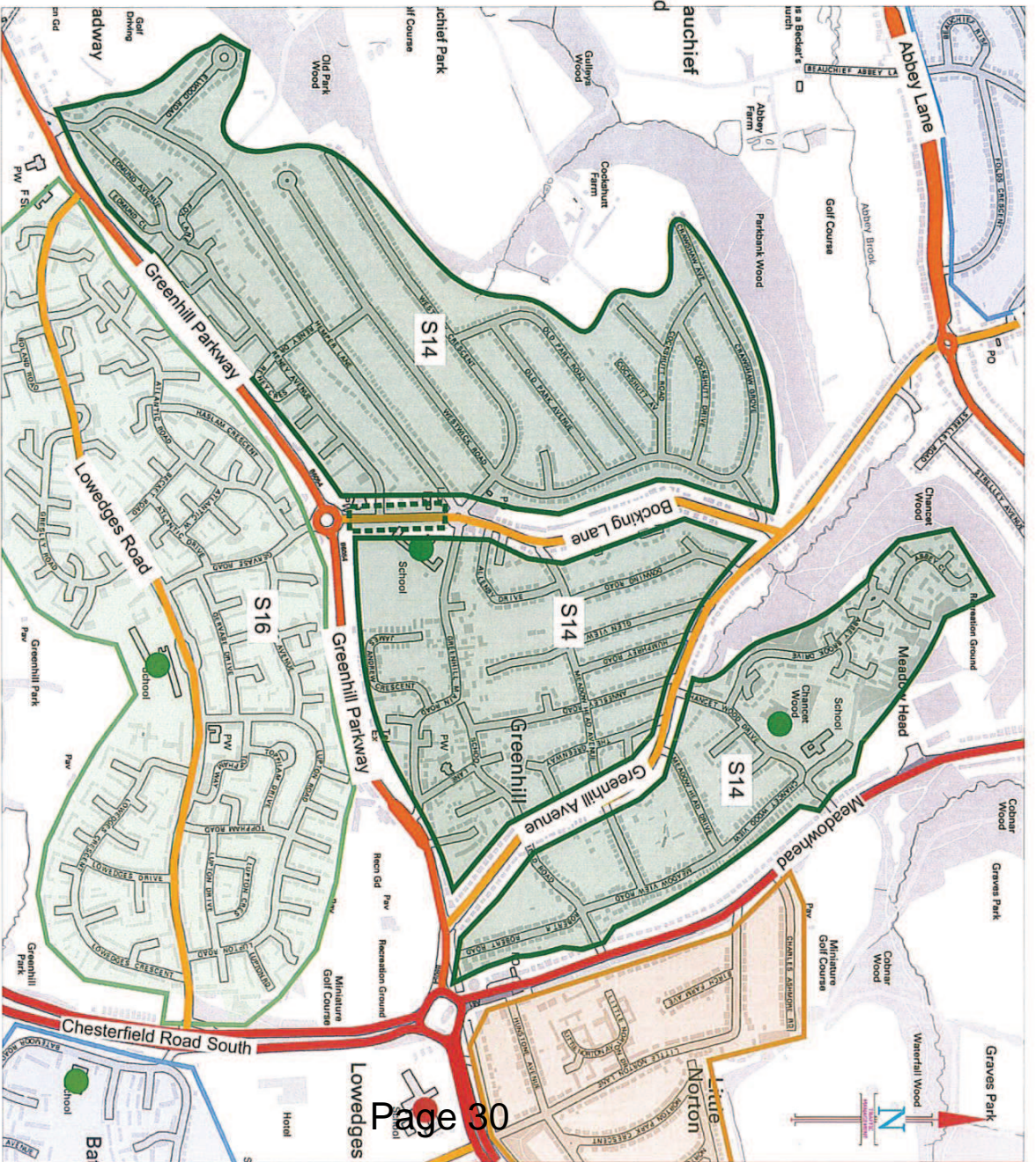
2015/16 Programme

Meadowhead, Greenhill & Beauchief (S14)

- Proposed 20mph area includes St Thomas Of Canterbury Catholic Primary School and Greenhill Primary School
- The existing 30mph limit would remain on Bocking Lane and Greenhill Avenue
- An advisory part-time 20mph limit would be introduced on Bocking Lane around the entrance to Greenhill Primary School, to operate at the start and end of the school day
- Speed data will be collected to confirm whether roads are suitable to be made subject to a 20mph speed limit

Key

	'A' road
	'B' road
	'C' road
	Primary, Junior and Infant schools
	Secondary schools
	Potential future 20mph speed limit
	Existing 20mph zone
	Existing 20mph speed limit
	2015-16 20mph speed limit programme
	Advisory part-time 20mph limit



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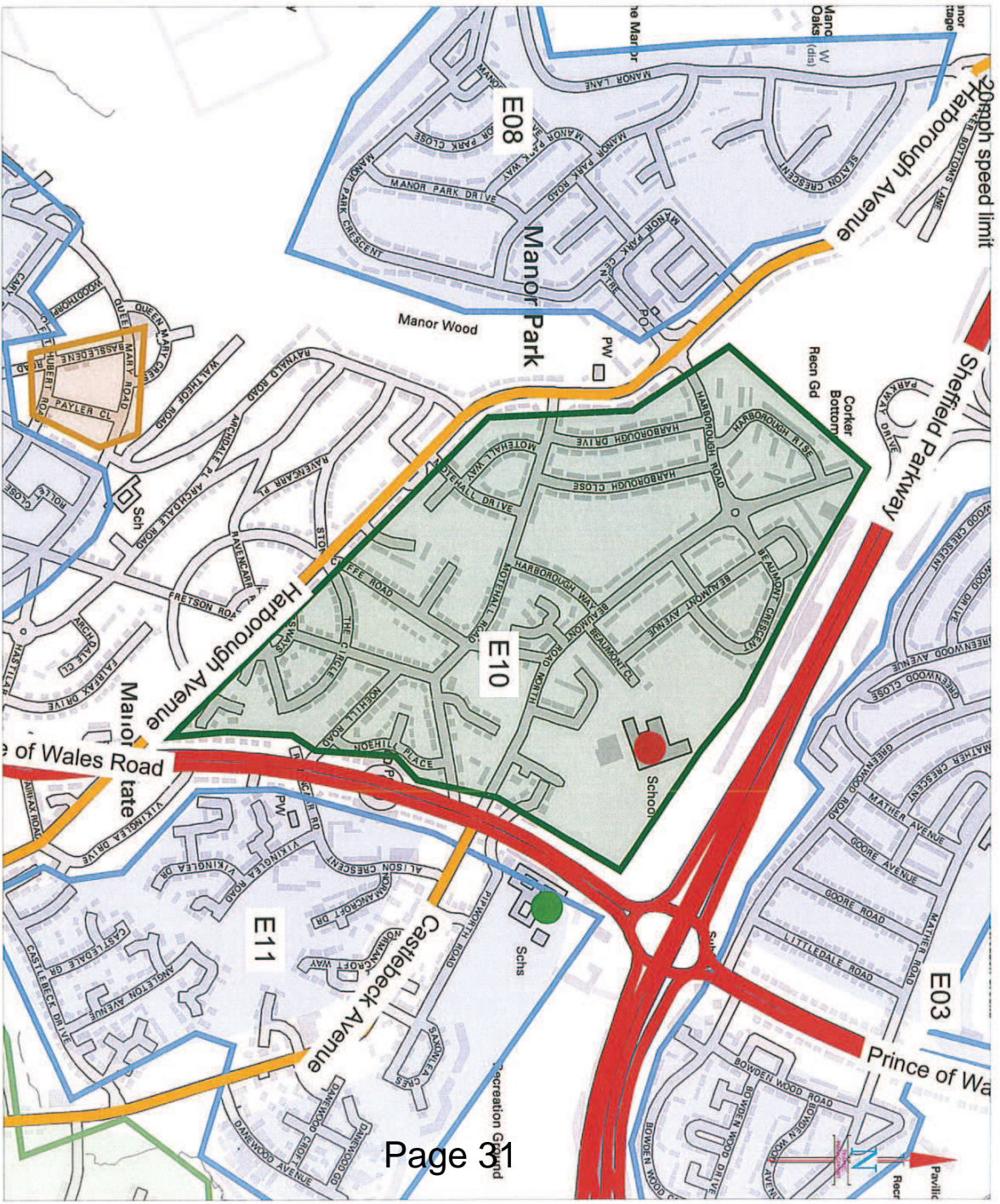
Sheffield 20mph Speed Limit Strategy 2015/16 Programme

Park Academy (E10)

- Proposed 20mph area bounded by, but not including, Harbrough Avenue, and Prince of Wales Road
- Area includes, and would be part-funded by Park Academy
- Speed data will be collected to confirm whether roads are suitable to be made subject to a

Key

	'A' road
	'B' road
	'C' road
	Primary, Junior and Infant schools
	Secondary schools
	Potential future 20mph speed limit
	Existing 20mph zone
	Existing 20mph speed limit
	2015-16 20mph speed limit programme



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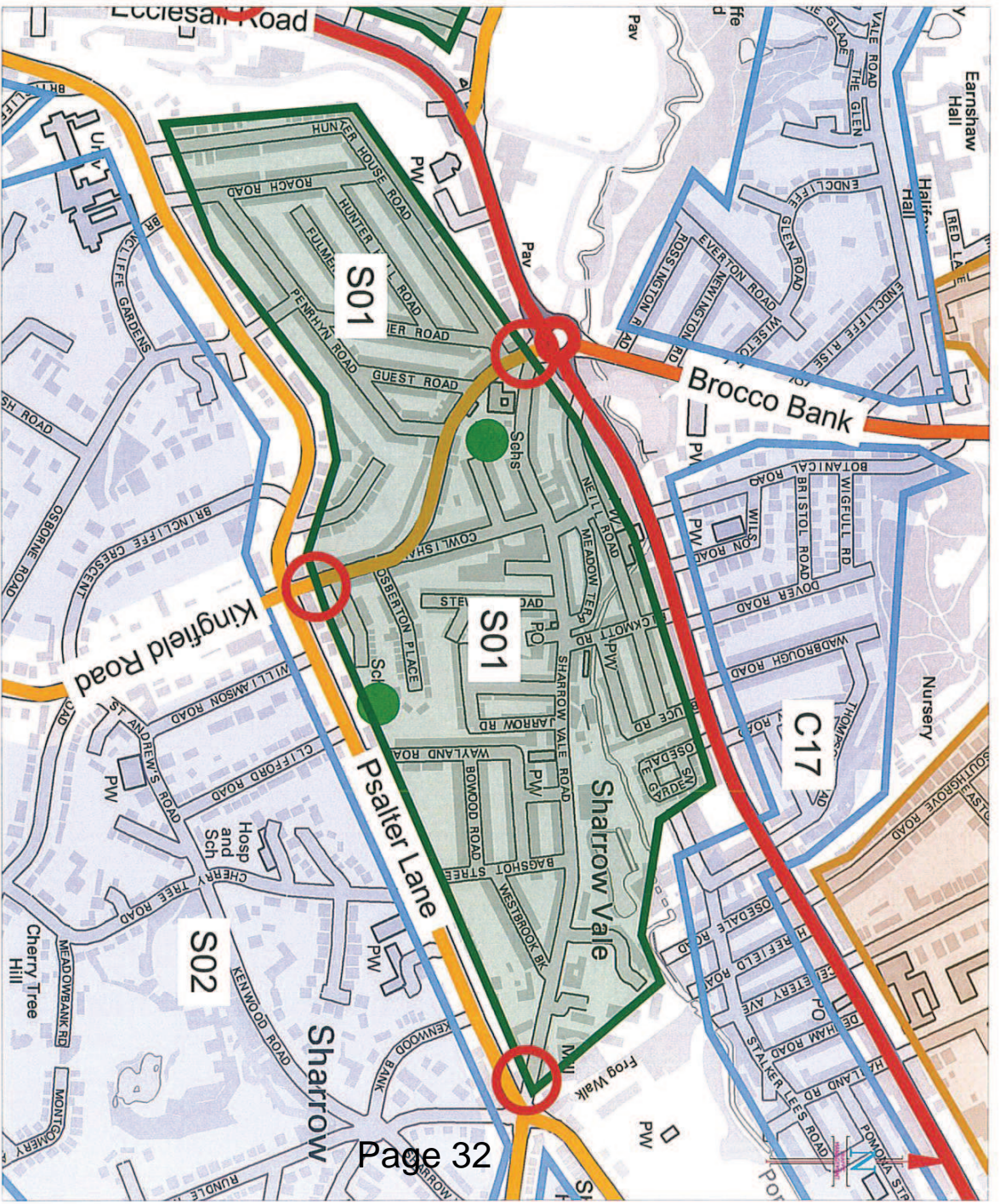
Sheffield 20mph Speed Limit Strategy 2015/16 Programme

Sharrowvale (S01)

- Proposed 20mph area to be bounded by, but not include, Ecclesall Road and Psalter Lane and would include Hunters Bar Infant and Junior School
- Speed data will be collected to confirm whether roads are suitable to be made subject to a 20mph speed limit

Key

- A' road
- B' road
- C' road
- Primary, Junior and Infant schools
- Secondary schools
- Potential future 20mph speed limit
- Existing 20mph zone
- Existing 20mph speed limit
- 2015-16 20mph speed limit programme
- Entry point with half-width red surfacing



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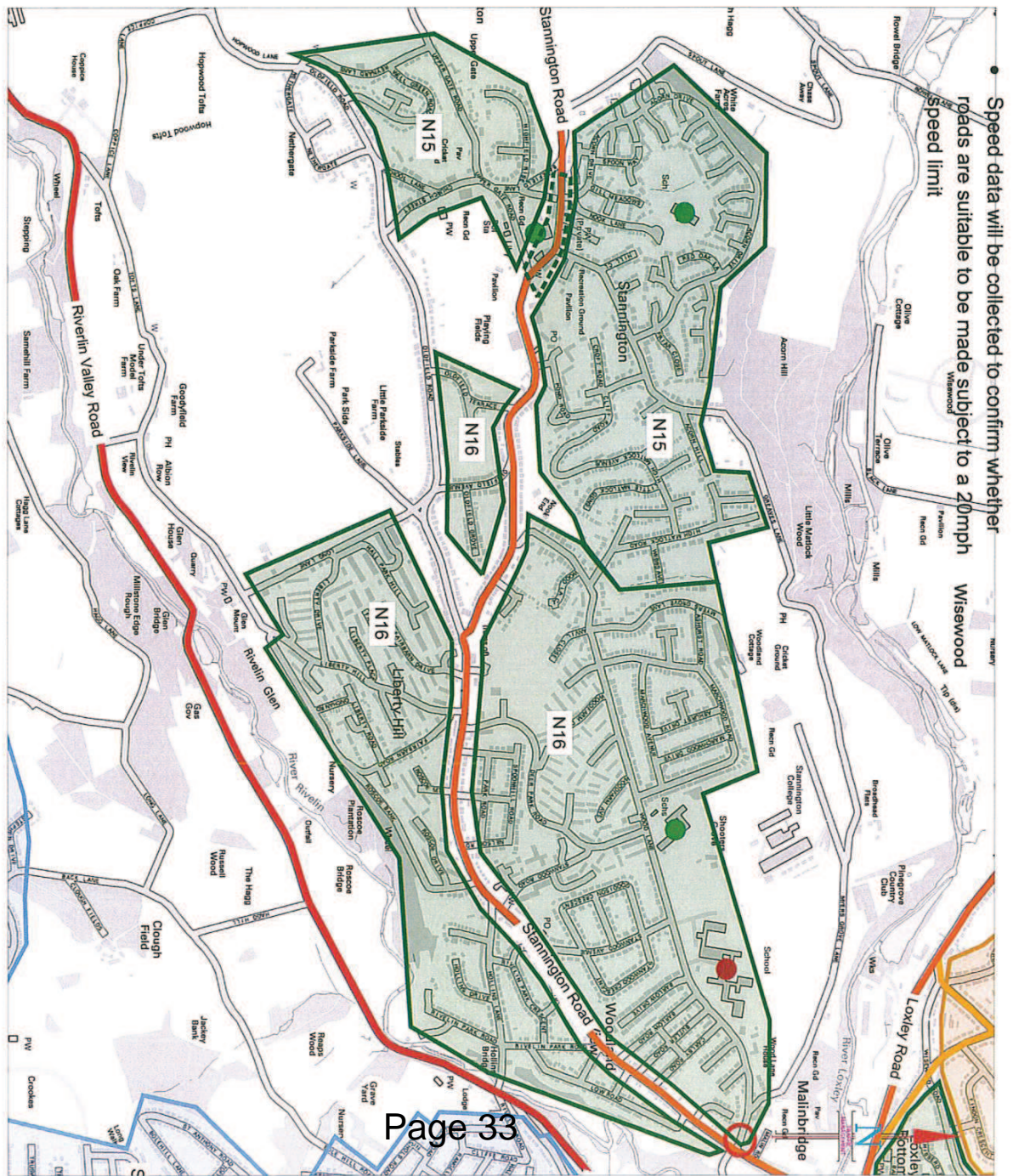
Sheffield 20mph Speed Limit Strategy

2015/16 Programme

Stannington (N15 & N16)

- Proposed 20mph area includes Forge Valley Community School, Shooters Grove Primary School and Nook Lane Junior School
- An advisory part-time 20mph limit would be introduced on Stannington Road around the entrance to Stannington Infant School to operate at the start and end of the school day
- The existing 30mph limit would remain on Stannington Road (B6076)

- Key**
- 'A' road
 - 'B' road
 - 'C' road
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 - Potential future 20mph speed limit
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 - Existing 20mph speed limit
 - 2015-16 20mph speed limit programme
 - Advisory part-time 20mph limit
 - Entry point with half-width red surfacing



Speed data will be collected to confirm whether roads are suitable to be made subject to a 20mph speed limit

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Sheffield 20mph Speed Limit Strategy

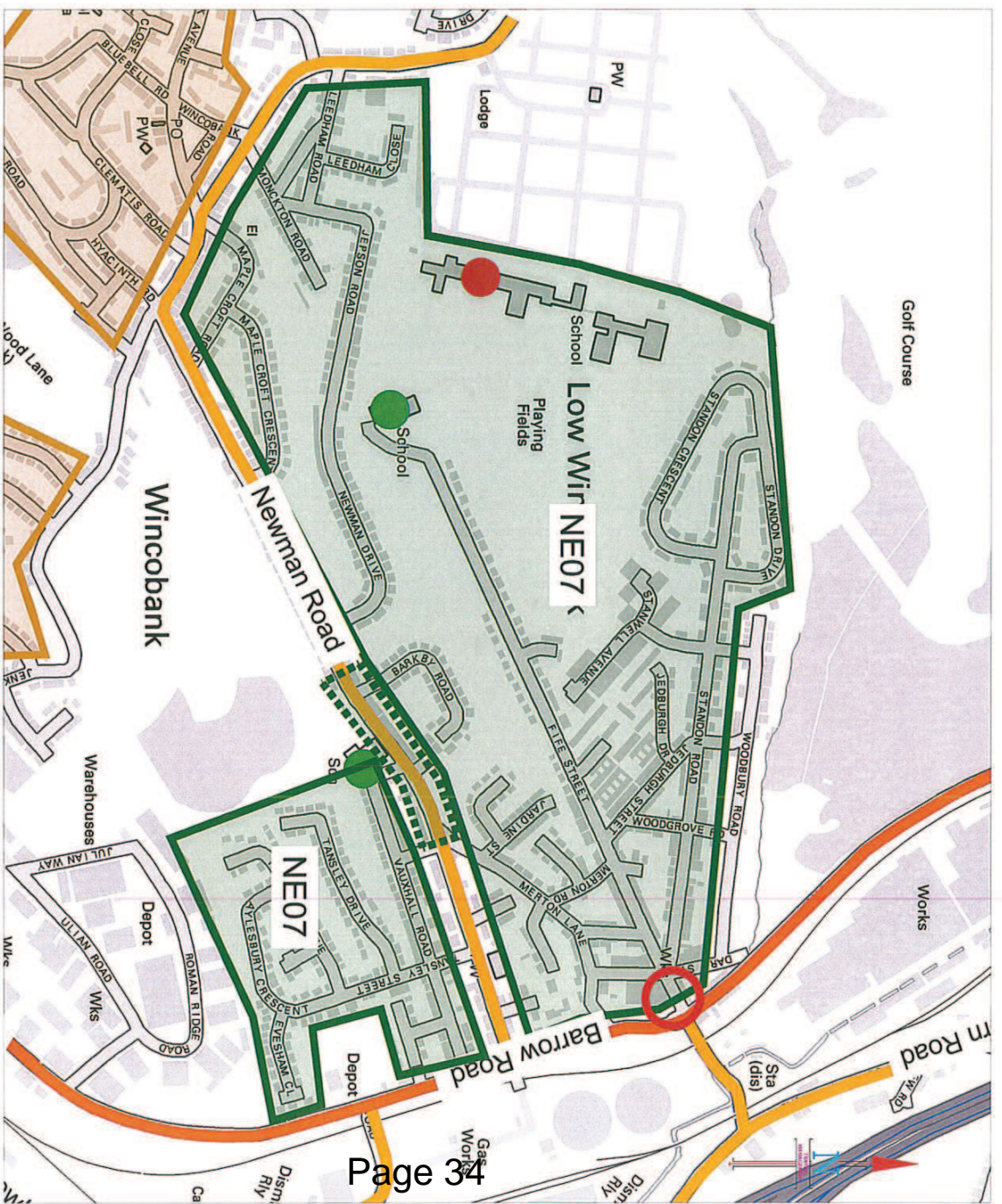
2015/16 Programme

Wincobank (NE07)

- Proposed 20mph area includes Concorde Junior School and Hinde House 3-16 School
- The existing 30mph limit would remain on Newman Road
- An advisory part-time 20mph limit would be introduced on Newman Road around the entrance to Wincobank Nursery and Infant School, to operate at the start and end of the school day
- Speed data will be collected to confirm whether roads are suitable to be made subject to a 20mph speed limit

Key

- 'A' road
- 'B' road
- 'C' road
- Primary, Junior and Infant schools
- Secondary schools
- Potential future 20mph speed limit
- Existing 20mph zone
- Existing 20mph speed limit
- 2015-16 20mph speed limit programme
- Advisory part-time 20mph limit
- Entry point with half-width red surfacing



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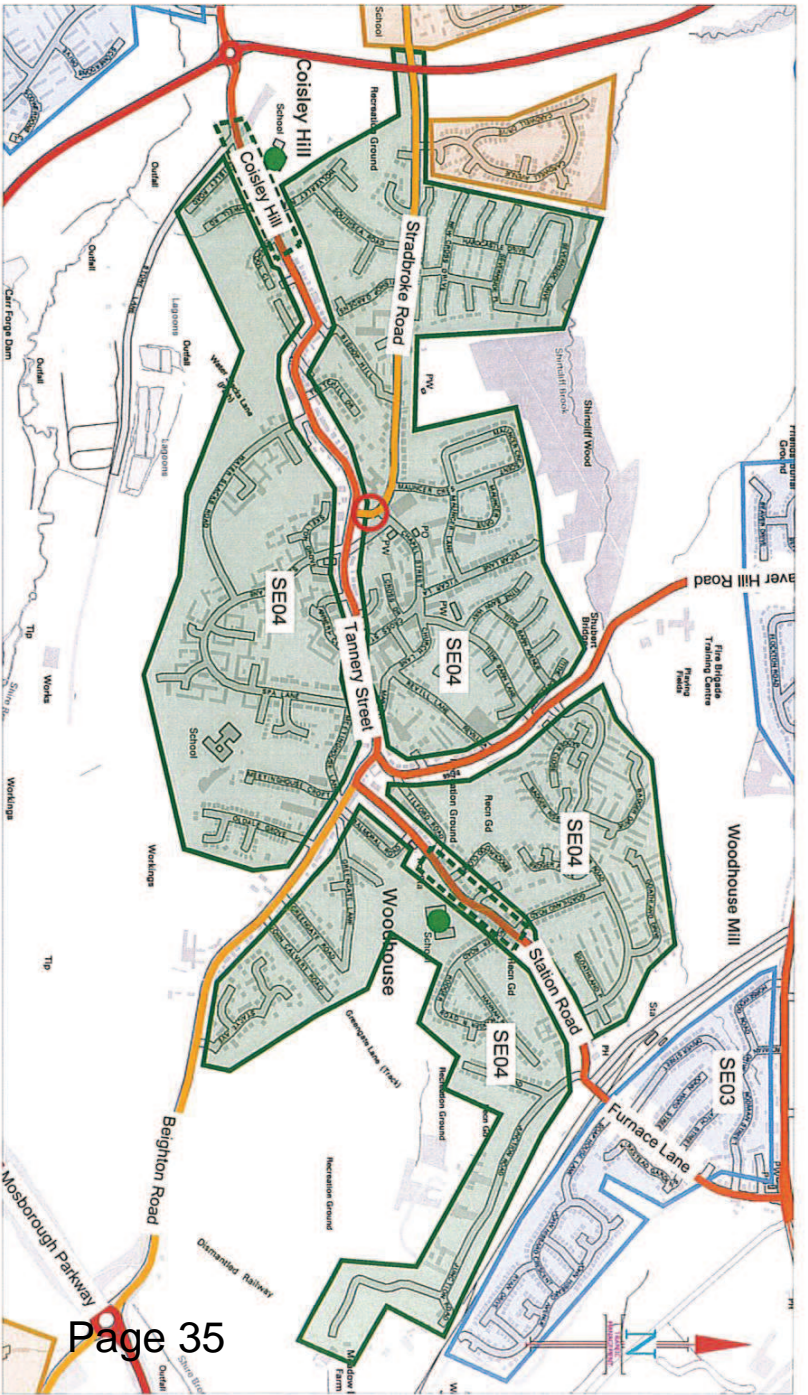
- Key**
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 - 'B' road
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 - Primary, Junior and Infant schools
 - Secondary schools
 - Potential future 20mph speed limit
 - Existing 20mph zone
 - Existing 20mph speed limit
 - 2015-16 20mph speed limit programme
 - Advisory part-time 20mph limit
 - Entry point with half-width red surfacing

Sheffield 20mph Speed Limit Strategy 2015/16 Programme

Woodhouse (SE04)

- The 20mph limit would tie into existing 20mph zones

- The existing 30mph limit would remain on Coisley Hill, Tannery Street, Station Road and Beaver Hill Road.



- An advisory part-time 20mph speed limit would be introduced around the Coisley Hill entrance to Woodhouse West Primary School and the Station Road entrance to Brunswick Community Primary School, to operate at the start and end of the school day
- Speed data will be collected to confirm whether roads are suitable to be made subject to a 20mph speed limit

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SHEFFIELD CITY COUNCIL Individual Cabinet Member Decision

Report of: Executive Director, Place

Date: 8 January 2015

Subject: Green Routes Scheme – Deep Lane/Grange Lane,
improved crossing point

Author of Report: James Haigh, 2736176

Summary:

This report describes the proposed changes to improve the perception of safety for users of the Blackburn Valley Cycle Route when crossing Deep Lane.

It also sets out officer's response to the objections to the scheme.

Reasons for Recommendations:

Officers believe the objections have been addressed and the reasons for the recommendations outweigh the objections received. The build-outs are essential to improve visibility and reduce the crossing distance at this location.

Recommendations:

To approve the scheme as described in the report.

Inform the objectors accordingly.

Background Papers: Appendix A – 1697, Consultation letter and plan
Appendix B – 1697, Modified plan showing track runs

Category of Report: OPEN

Statutory and Council Policy Checklist

Financial Implications
Cleared by: Gaynor Saxton
Legal Implications
Cleared by: Nadine Wynter
Equality of Opportunity Implications
Cleared by: Annemarie Johnston
Tackling Health Inequalities Implications
NO
Human rights Implications
NO:
Environmental and Sustainability implications
NO
Economic impact
NO
Community safety implications
NO
Human resources implications
NO
Property implications
NO
Area(s) affected
Shiregreen
Relevant Cabinet Portfolio Leader
Leigh Bramall
Relevant Scrutiny Committee if decision called in
Culture, Economy and Sustainability
Is the item a matter which is reserved for approval by the City Council?
NO
Press release
NO

GREEN ROUTES SCHEME – DEEP LANE/GRANGE LANE, IMPROVED CROSSING POINT

1.0 SUMMARY

1.1 This report describes the proposed changes to improve the perception of safety for users of the Blackburn Valley Cycle Route when crossing Deep Lane.

1.2 It also sets out the response to an objection to the scheme.

2.0 WHAT DOES THIS MEAN FOR SHEFFIELD PEOPLE?

2.1 Increased inter-visibility at the crossing will help improve safety for all users and contribute to the development of a high quality Green Routes Network for cyclists, pedestrians, and in this location equestrians.

2.2 The improvement to the crossing will contribute to the “A Great Place to Live” theme of the Corporate Plan by encouraging sustainable modes of transport.

3.0 OUTCOME AND SUSTAINABILITY

3.1 It is anticipated that once the proposals are in place they will help improve visibility for users of the Blackburn Valley Cycle Route as well as highlight the crossing to drivers and help reduce vehicle speed.

4.0 REPORT

Introduction

4.1 This scheme is designed to provide a safer crossing of Deep Lane with improved visibility for all users, in particular non confident cyclists, family groups and disabled pedestrians, using the Blackburn Valley Cycle Route as part of the Sheffield Green Routes Network.

4.2 The area surrounding the crossing point mainly consists of heavy commercial and light industrial premises with the associated traffic including some HGV's.

4.3 Whilst being subject to a 30mph speed limit the crossing is approximately 150m away from a length of national speed limit road and higher than intended speeds have been observed

4.4 Deep Lane is used as a commuter route to and from Rotherham as it can be used to bypass potential queuing traffic at Chapeltown and Meadowhall. This results in heavy traffic flows at peak times.

4.5 The proposals as shown in appendix A aim to narrow Deep Lane at the point of the crossing which will carry several benefits for users:

- It will increase the visibility by encouraging users to stand further forward whilst crossing.
- The build-outs will reduce the width of carriageway meaning the

users have a shorter distance to cross.

- By narrowing the carriageway it will change the feel of that section of carriageway which in turn will help to reduce speed.

Public consultation

- 4.9 During November 2014 26 letters were delivered to fronting property owners (see appendix A).
- 4.10 Two objections have been received from businesses who were concerned that the scheme would make the road too narrow and restrict access by large vehicles to at least one business.
- 4.12 The businesses made the following points:
- The scheme would cause a bottle neck for traffic and cause tailbacks.
 - They would not be able to receive deliveries from a car transporter.
 - 'Cutting the trees down near the bridge would be a better and a cheaper option as the cyclists would then be able to have a clearer vision of the road and oncoming traffic'

Officer response

- 4.13 Currently the bridge to the South of the crossing is 4.2m wide which is much narrower than the 5.5m originally proposed for the crossing point. 5.5m is wide enough to pass two cars.
- 4.14 Following the concerns over access by a car transporter officers modified the plan (see Appendix B) which increased the radius of one of the build-out kerbs to 7m and also increased the width of the road to 6m.
- 4.15 As part of the scheme Amey will be asked to carry out a programme of maintenance in the immediate area surrounding the crossing point including heavy pruning of trees. On its own this would not provide the required results.

Other Consultations

- 4.18 Local Members, South Yorkshire Police, the Fire & Ambulance services and Veolia have been consulted about the scheme. No objections have been received from them.

Relevant Implications

- 4.19 The budget estimate for the construction of this scheme is £45,000. Financial approval is conditional upon the approval by EMT on the 09 December 14 of the Capital Approval Form (CAF) variation for the Green Routes Network project (92914) which has been submitted and the allocation of an additional £45,000 Local Sustainable Transport Plan capital funding at SLG / SCR Transport Committee to the project. The Green Routes Network project budget would then cover the cost of scheme construction.

The Commuted Sum for the scheme has been estimated at £22,500 (50% of the construction cost as recommended for build out and tactile schemes). This revenue contribution will be funded by a reduction in the Local Transport Plan approved capital allocation for the Green Routes Network project in 2014/15, as set out in the CAF variation noted above.

Should the SLG/SCR Transport Committee fail to allocate additional Local Sustainable Transport Plan capital funding to the scheme in 2014/15 the additional funding required will be included within the Sustainable Transport Exemplar programme due to start in 2015/16, and run for three years, in addition to the Green Routes Network project budget for 2014/15. Financial approval subject to confirmation of next year's programme.

- 4.20 No equality implications have been identified and the proposals to improve crossing facilities are equality neutral affecting all local people equally regardless of age, sex, race, faith, disability, sexuality, etc
- 4.21 Legal implications - the Council, as the Highway Authority for Sheffield, has powers under Part V of the Highways Act 1980 to implement the improvements requested in this report. As the Traffic Authority the Council also has similar powers under the Road Traffic Regulation Act 1984 and in exercising that power the Council must be satisfied that it will secure the expeditious, convenient and safe movement of vehicular and other traffic (including pedestrians). Provided the Council is satisfied that this is the case, it is acting lawfully and within its powers.

The Council has received a number of objections in response to the consultation from individuals who are not supportive of the approach the Council is taking with regard to the proposals outlined in this report. The Council therefore needs to consider whether the benefits of implementing these proposals outweigh the objections. Provided the Council is satisfied that this is the case, it is acting lawfully and within its powers.

5.0 ALTERNATIVE OPTIONS CONSIDERED

- 5.1 A signing only scheme was considered but it was decided that it would not have sufficient effect on driver speed and would not have any effect on visibility.

6.0 REASONS FOR RECOMMENDATIONS

- 6.1 Officers believe the objections have been addressed and the reasons for the recommendations outweigh the objections received. The works described in this report will contribute to an improvement in safety on Deep Lane.

7.0 RECOMMENDATIONS

- 7.1 To approve the scheme as described in the report.
- 7.2 Inform objectors accordingly.

Executive Director, Place

Regeneration and Development Services

Director: David Caulfield, RTPi
Scheme Design · 2-10 Carbrook Hall Road · Sheffield · S9 2DB
James.Haigh@sheffield.gov.uk

Officer: James Haigh
Ref: SD-1697-JH02

Tel: (0114) 273 6176
Date: 31st October 2014

The Occupier

Dear Sir/Madam

Green Routes Scheme – Deep Lane/Grange Lane, improved crossing point

The City Council is aiming to develop a number of cycle routes throughout the city to improve conditions not only for cyclists but for pedestrians and wheelchair users. One such facility identified for improvement is the Deep Lane/Grange Lane crossing point which is connected to the Blackburn Valley Cycle Route.

Enclosed with this letter is a plan showing the proposals for the crossing improvements, the intention is to narrow the road at this point which will improve the visibility for all users and also reduce the distance which they have to cross. (Please note, the road at this point will still be wide enough to pass two cars).

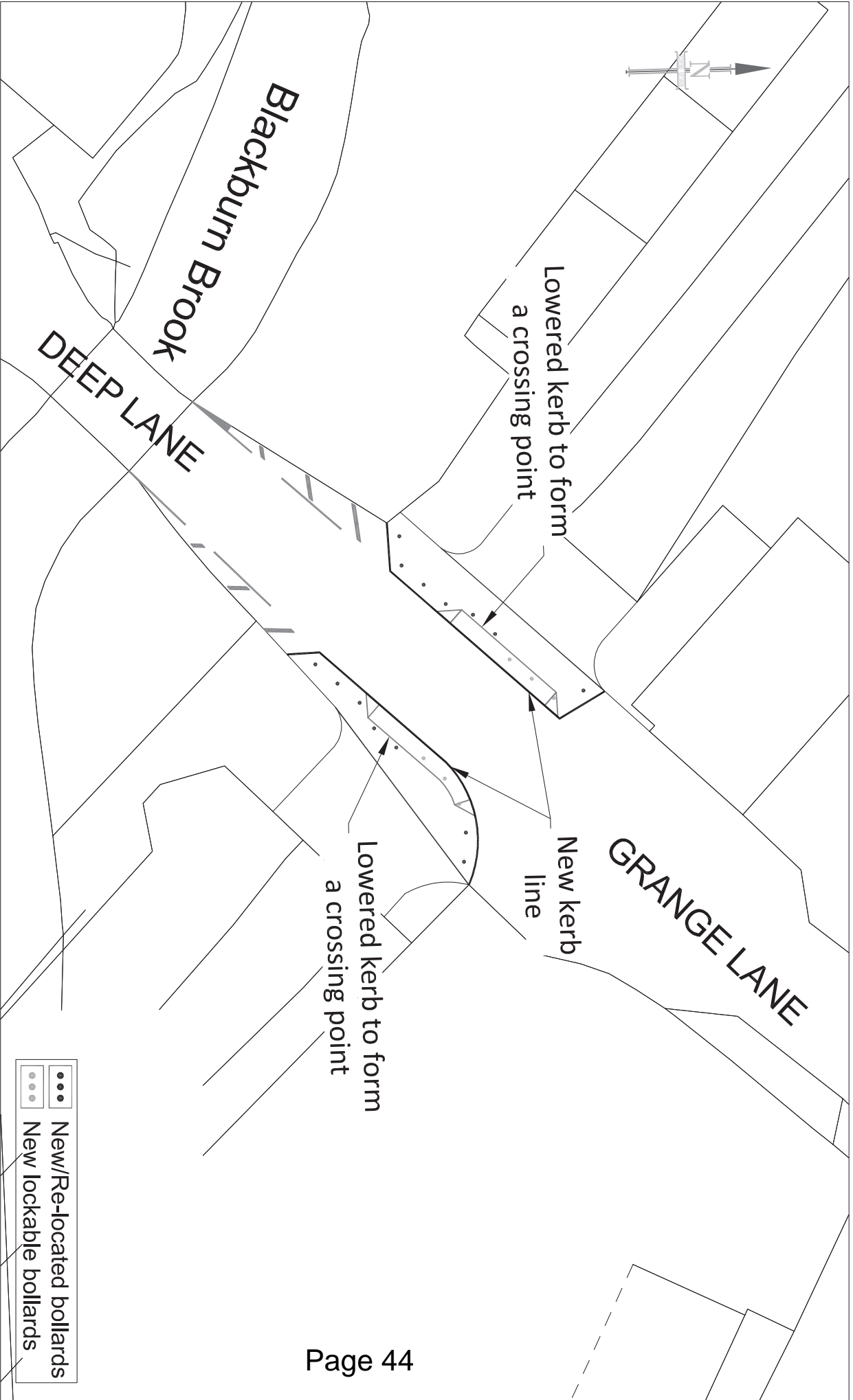
Although the main aim is to improve conditions for users it is also hoped that the reduced road width will help slow drivers as they pass through the area, which in turn will improve the situation for all local residents/business owners.

I hope that the attached plan is self-explanatory. However, if you need any further assistance please feel free to contact me. Please note that the consultation period ends 21st November 2014, please let me have your support or objections before this time.

Yours faithfully



James Haigh
Technician, Scheme Design
Transport, Traffic and Parking Services



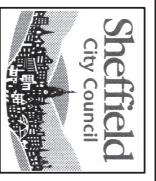
Green Routes Scheme

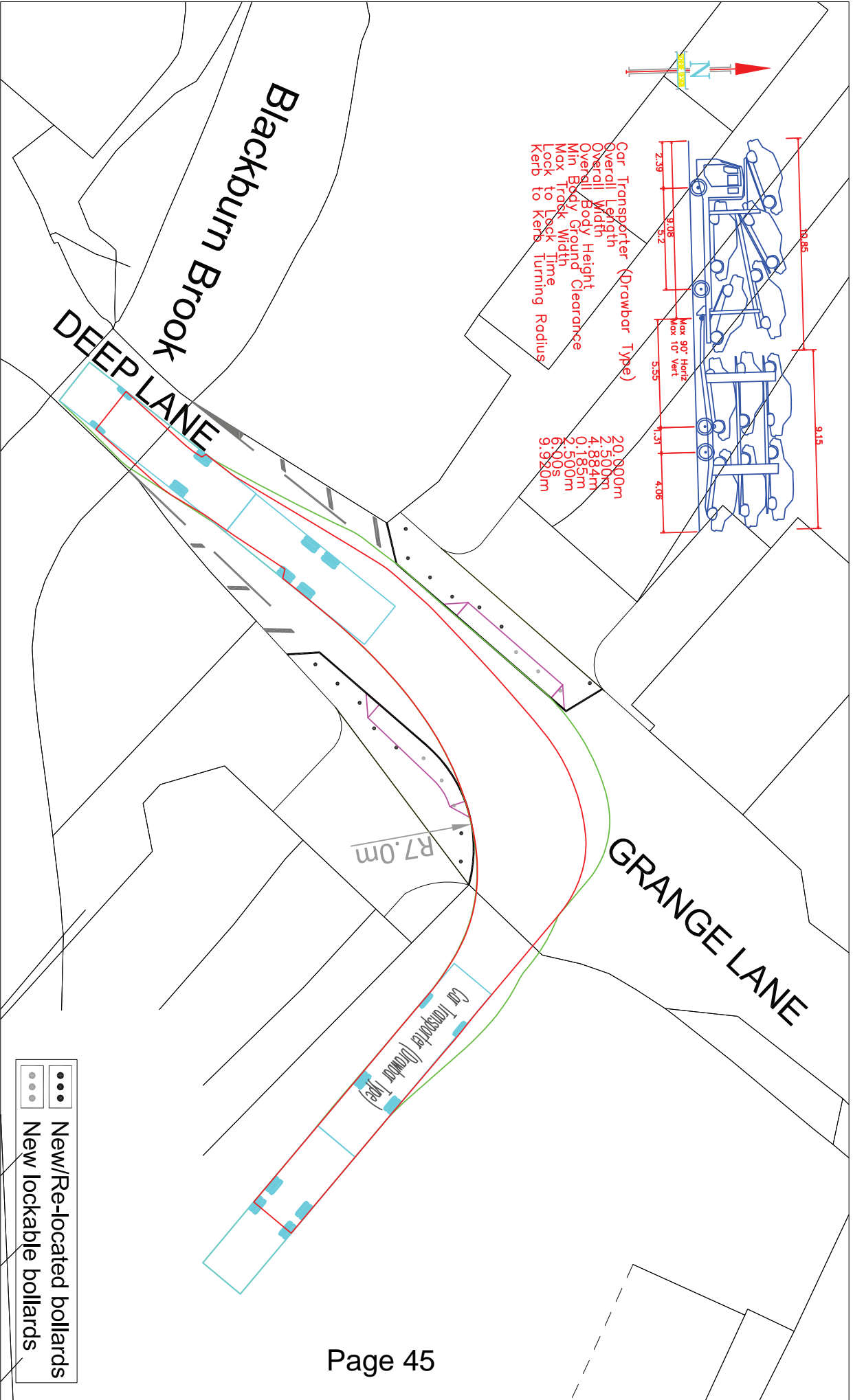
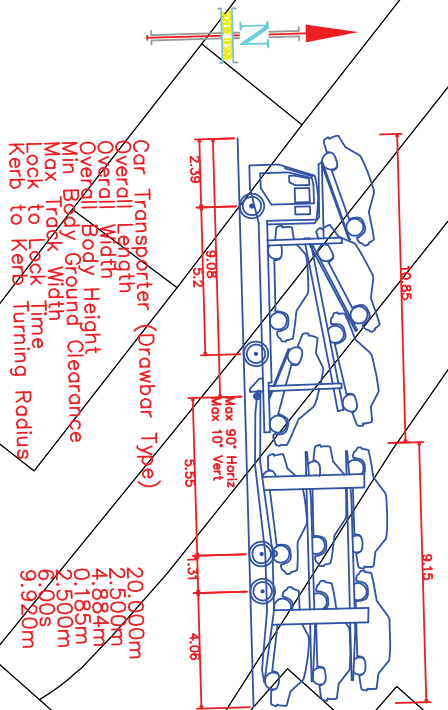
Deep Lane, Cycle Crossing

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- New/Re-located bollards
- New lockable bollards

James Halpin, Scheme Design
 Transport, Traffic & Parking Services
 Oct 2014
 Paper size: A4
 Scale: NTS
 Drawing No: SD-1697-C01





- New/Re-located bollards
- New lockable bollards

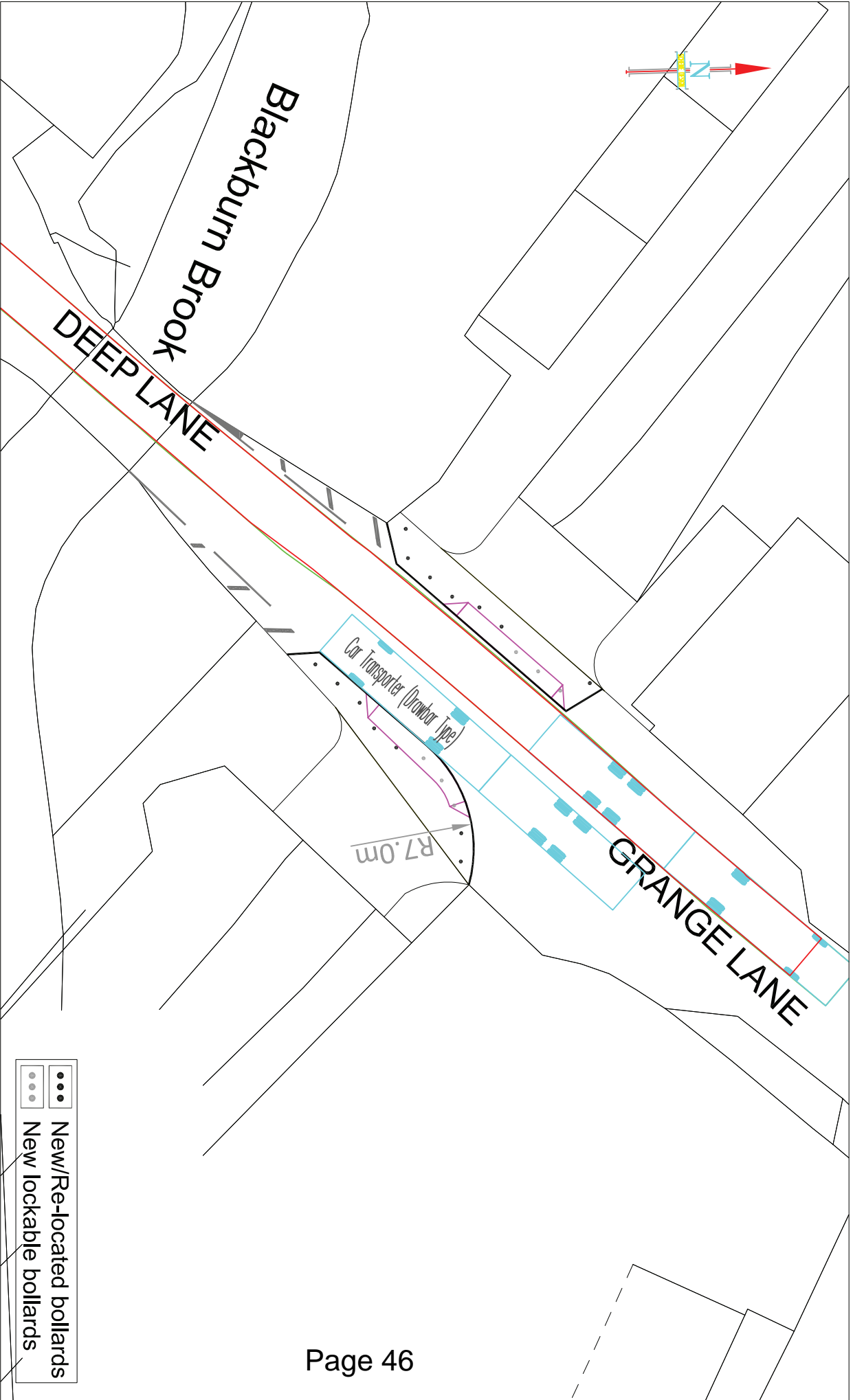
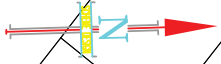
James Halgh, Scheme Design
 Transport, Traffic & Parking Services
 Nov 2014
 Paper size: A4
 Scale: NTS
 Drawing No: SD-1697-C01.2



Green Routes Scheme

Deep Lane, Cycle Crossing



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Green Routes Scheme

Deep Lane, Cycle Crossing

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-  New/Re-located bollards
-  New lockable bollards

James Halgh, Scheme Design
 Transport, Traffic & Parking Services
 Nov 2014
 Paper size: A4
 Scale: NTS
 Drawing No: SD-1697-C01.3





SHEFFIELD CITY COUNCIL Individual Cabinet Member Decision

Report of: Executive Director, Place

Date: 8 January 2015

Subject: Sheffield 20mph Speed Limit Strategy:
Consultation feedback to the proposed introduction of a
20mph speed limit in Hackenthorpe and the area around
London Road

Author of Report: Simon Nelson, 2736176

Summary:

This report describes the response from residents to the proposal to introduce a 20mph speed limit in Hackenthorpe and the area around London Road, reports the receipt of objections and sets out the Council's response.

Reasons for Recommendations:

Reducing the speed of traffic in residential areas will, in the long term, reduce the number and severity of accidents, reduce the fear of accidents, encourage sustainable modes of travel and contribute towards the creation of a more pleasant, cohesive environment.

Having considered the objections to the introduction of a 20mph speed limit in Hackenthorpe and the area around London Road the officer view is that the reasons set out in this report for making the Speed Limit Order outweigh the objections. The introduction of a 20mph speed limit in these areas would be in-keeping with the City's approved 20mph Speed Limit Strategy.

Recommendations:

- 7.1 Make the Hackenthorpe and London Road area 20mph Speed Limit Orders in accordance with the Road Traffic Regulation Act 1984.
- 7.2 Inform the objectors accordingly.
- 7.3 Introduce the proposed 20mph speed limits.

7.4 Introduce an advisory part-time 20mph speed limit on parts of Beighton Road as shown in Appendix C.

Background Papers: Appendix A: Hackenthorpe consultation leaflet
Appendix B: Sheffield Road/Beighton Road letter
Appendix C: Proposed part-time advisory 20mph speed limit, Beighton Road
Appendix D: London Road consultation leaflet

Category of Report: OPEN

Statutory and Council Policy Checklist

Financial Implications
YES Cleared by: Damian Watkinson
Legal Implications
YES Cleared by: Nadine Winter
Equality of Opportunity Implications
YES Cleared by: Ian Oldershaw
Tackling Health Inequalities Implications
NO
Human rights Implications
NO:
Environmental and Sustainability implications
NO
Economic impact
NO
Community safety implications
NO
Human resources implications
NO
Property implications
NO
Area(s) affected
Beighton, Birley and Central
Relevant Cabinet Portfolio Leader
Jayne Dunn
Relevant Scrutiny Committee if decision called in
Culture, Economy and Sustainability
Is the item a matter which is reserved for approval by the City Council?
NO
Press release
YES

SHEFFIELD 20MPH SPEED LIMIT STRATEGY: CONSULTATION FEEDBACK TO THE INTRODUCTION OF A 20MPH SPEED LIMIT IN HACKENTHORPE AND THE AREA AROUND LONDON ROAD

1.0 SUMMARY

1.1 This report describes the response from residents to the proposal to introduce a 20mph speed limit in Hackenthorpe and the area around London Road, reports the receipt of objections and sets out the Council's response.

2.0 WHAT DOES THIS MEAN FOR SHEFFIELD PEOPLE?

2.1 Reducing the average speed of drivers in residential areas is expected, over time, to bring about a reduction in the number and severity of traffic accidents, thus helping to create *safe and secure communities*. Implementing the 20mph speed limits described in this report together with an ongoing programme of publicity and driver education would contribute to the creation of a safer residential environment and a *Great Place to Live*. The response to the consultation contributes to the *working better together* value of the Council Plan *Standing up for Sheffield*.

3.0 OUTCOME AND SUSTAINABILITY

3.1 These schemes represent a step towards influencing driver behaviour and establishing 20mph as the default maximum appropriate speed in residential areas. This will contribute to the delivery of:

- the '*sustainable and safe transport*' objective of the Corporate Plan;
- Policy W of the Sheffield City Region Transport Strategy 2011-2026 (*To encourage safer road use and reduce casualties on our roads*);
- the Council's Vision For Excellent Transport In Sheffield (*a better environment; a healthier population; a safer Sheffield*); and
- the Fairness Commission's recommendation for a 20mph speed limit on all residential roads in Sheffield.

4.0 REPORT

Introduction

4.1 In February 2011, Full Council adopted the following motion: "*To bring forward plans for city-wide 20mph limits on residential roads (excluding main roads)*". This led to the adoption of the Sheffield 20mph Speed Limit Strategy by the Cabinet Highways Committee on 8th March 2012, the long-term aim of which is to establish 20mph as the maximum appropriate speed in residential areas of Sheffield¹. The first seven areas were introduced during the financial

¹ [Sheffield City Council - Meeting of Cabinet Highways Committee on Thursday 8 March 2012](#)

years 2013/14 and 2014/15. These speed limits are indicated by traffic signs and road markings only, that is, they would not include any additional 'physical' traffic calming measures such as road humps.

- 4.2 The programme for 2014/15 was approved by the Highway Cabinet member on 6 February 2014 and comprised seven areas. Hackenthorpe and the area around London Road are the last two within this year's programme to be submitted for approval for introduction this financial year.
- 4.3 The intention to introduce a 20mph Speed Limit Order in Hackenthorpe and the area around London Road has now been advertised. The responses received are set out below. All written comments are available to view on request.

The introduction of a 20mph Speed Limit in Hackenthorpe

- 4.4 Approximately 3250 properties have received a leaflet informing about the intention to introduce a 20mph limit (see Appendix A).
- 4.5 19 people have written or telephoned to express their support for the 20mph limit. One resident has objected:

"It is my view that these proposals are wholly disproportionate response to any perceived issues with the current speeds limits. 30mph is perfectly adequate for this residential area any restrictions would be almost impossible to monitor/police."

- 4.6 Sheffield Road and Beighton Road, one of two through routes that divide the area (the other being Birley Spa Lane) were originally omitted from the potential 20mph area. Local ward members felt that residents should be consulted on their inclusion. A further letter was delivered to all properties in the area explaining this additional proposal (see Appendix B).
- 4.7 28 people have contacted the council to support the inclusion of Sheffield Road and Beighton Road.

"I feel it would be a much safer option for the numbers of children who have to walk this route to and from school or reach public transport in order to travel to school. As a resident overlooking Sheffield Rd I also feel it would make a great improvement to the surrounding environment."

The Cycle Touring Club has indicated their support for this proposal.

- 4.8 Eight objections have been received. Many people, both for and against, feel that traffic calming and enforcement would also be needed if drivers are to slow down.
- 4.9 Six people, including five of the objectors suggested that a part-time, localised 20mph limit centring on the entrance to Rainbow Forge school would be more appropriate than a blanket 20mph limit on Sheffield Road and Beighton Road.

- 4.10 The head teacher at Rainbow Forge Primary School has made the following statement:

“As a school with a main entrance on to Beighton Road, we fully support the proposed 20mph speed limit. The road is a bus route and is very busy at peak hour times. Our parents park on the road as we have no parking area, so this adds to the congestion during school drop off and pick up. During this time there have been numerous near misses or minor accidents and the speed of drivers is usually the cause.

Not only are parents trying to safely help their children in and out of the parked cars, we also have a number of families and older children who walk along the road, or who need to cross the road on their journey home. Our most recent ‘travel to school survey’ (January 2012) showed that the vast majority of our pupils (71%) walk to school. A lower speed limit, both on Beighton Road and in the wider Hackenthorpe area, would make this journey safer for our children and families, reducing anxiety for parents and help to ensure that no one is injured. We hope that this will help encourage more pupils and parents to walk to school in future.

- 4.11 South Yorkshire Police has formally objected to the inclusion of Beighton Road and Sheffield Road:

“[Available speed data] shows that the speeds on Beighton Road and Sheffield Road do not fall within the DFT recommended guidelines of 24mph. In fact all three sets old data show that well over 50% of vehicles exceed the current 30mph speed limit on this road with the 85th %ile travelling 37mph/38mph.

There is obviously a long term issue with speed on this road. I also noticed that there is a vehicle activated sign on Beighton Road, which would confirm this. Within the guidance for setting local speed limits, it does state that where there is poor compliance with an existing speed limit on a road, or stretch of road, the reason for the non-compliance should be examined before a solution sought.

I visited the site last week and followed vehicles travelling up and down the road. The speed of the vehicle in front going uphill from Moss Way was in the region of 32 to 35mph. However, vehicles travelling down the bottom section towards Moss Way were in excess of 50mph. I would say, that the road layout on this bottom section of Beighton Road did not make this speed feel uncomfortable at that time.

As this proposed extension to the 20mph zone is unlikely to be self-enforcing without some major engineering works, and does not fall anywhere near the DFT recommended guidelines of 24mph, and that to achieve compliance there should be no expectation on the police to provide additional enforcement beyond their routine activity. I feel that we must formally object to this section of Sheffield Road and Beighton Road being included within the 20mph zone.”

- 4.12 The police have also reviewed the proposals for the remainder of the Hackenthorpe area and identified Main Street as a road on which they feel drivers' speeds may not reduce after the reduction of the signed 20mph limit. The speeds on Main Street will be monitored before and after implementation of the scheme. If in time speeds remain unaltered additional measures will be considered to improve compliance with the new limit.

Discussion

- 4.13 Whilst acknowledging that people contacting the council to express a view regarding the proposals for Sheffield Road and Beighton Road have generally been supportive, officers share the concerns of the police and objectors regarding its inclusion within the 20mph speed limit area.
- 4.14 The relevant Department for Transport guidance notes the importance of existing speeds when designating new speed limits: *"If the mean speed is already at or below 24 mph on a road, introducing a 20 mph speed limit through signing alone is likely to lead to general compliance with the new speed limit."* (DfT Circular 01/2013)

However it goes on to describe a variety of competing factors and demands that a local authority should take into consideration when considering altering a speed limit and advises that these may be weighted differently according to the particular location in question.

"Sometimes a decision about a road's primary or most important function needs to be taken... Fear of traffic can affect peoples' quality of life and the needs of vulnerable road users must be fully taken into account in order to further encourage these modes of travel and improve their safety. Speed management strategies should seek to protect local community life."

- 4.15 Officers have canvassed colleagues at comparable authorities to see how they interpret the DfT guidance regarding the existing average speeds. Responses have been received from Bristol and Nottingham City Councils. Neither authority would introduce a 20mph on to roads where existing average speeds exceed the recommended 24mph without complementary measures such as additional road signs and markings to help slow traffic.

On roads that speeds may be higher than recommended, we have stressed that the roads will need consideration for additional measures and without these speeds are unlikely to decrease.

Nottingham City Council

- 4.16 Speed limits should both reflect the character of the road to which they apply but be realistic. The existing speeds on Sheffield Road and Beighton Road are so far above the maximum recommended by the Department for Transport. In the absence of funding for complementary traffic calming measures a 20mph limit would be artificially low and likely to result in excessive abuse and bring 20mph limits in general into disrepute the existing speeds.

- 4.17 Whilst officers cannot recommend the introduction of a 20mph speed limit along the full length of Sheffield Road and Beighton Road, improving the safety of school children remains one of the key objectives of the Sheffield 20mph Speed Limit Strategy. The Strategy expressly provides for the introduction of a localised, part-time speed limit around the entrance to a school that is located on a road that is otherwise unsuitable for a 20mph speed limit. This is the approach that officers recommend – the introduction of a part time, advisory 20mph speed limit centred around the entrance to Rainbow Forge Primary School as shown in Appendix C.
- 4.18 There is no footway on the south side of Sheffield Road from Christchurch Hackenthorpe Church, east for a distance of approximately 100m. There is an opportunity to improve pedestrian accessibility of the area through the provision of a small build out at each end of the section without footway to make it easier and safer for people to cross to the footway on the north side of Sheffield Road. Officers will ask that consideration be given to funding the provision of these build outs from the Streets Ahead Enhancement block of the Local Transport Plan.

The introduction of a 20mph Speed Limit in the London Road area

- 4.19 Leaflets have been delivered to 1650 properties (Appendix D). Five people have contacted the Council, four in support:
- "Received a leaflet and am completely for it! I live on Fairbank Rd and drivers use it as a cut through speedway! Children play everywhere and I regularly see careless drivers race up the road, mostly taxi drivers!"*
- 4.20 One objection has been received. The objector feels that the scheme would not achieve anything and would be a waste of money. The objection was made in a telephone call; the resident was advised to put her objection in writing but this has not so.
- 4.21 South Yorkshire Police have reviewed the proposals for the London Road area and identified Alderson Road as a road on which they feel drivers' speeds may not reduce after the reduction of the signed 20mph limit. The speeds on Alderson Road will be monitored before and after implementation of the scheme. If in time speeds remain unaltered additional measures will be considered to improve compliance with the new limit.

Other Consultees

- 4.22 The Head of the Road Policing Group has issued the following statement on behalf of South Yorkshire Police:

"The South Yorkshire Safer Roads Partnership has worked hard to achieve significant reductions in the numbers of collisions on our local roads. We have achieved all our agreed targets in reducing the number of people who are killed or seriously injured over the last few years however, we know that this success brings little comfort to the individuals, friends and families of those who are victims of such collisions."

It is well known that speed is a primary cause of collisions that result in death or serious injury and pedestrians and cyclists are the most vulnerable road users when in the presence of speeding vehicles. Within our local residential areas we know that the collision rates, when these factors come into play, are too high and need to be addressed.

South Yorkshire Police working alongside their colleagues in the Safer Roads partnership shares the clear commitment to address the causes of collisions and support new initiatives that help to achieve this goal.”

- 4.23 No response has been received from South Yorkshire Fire and Rescue Service or the Yorkshire Ambulance Service.
- 4.24 South Yorkshire Passenger Transport Executive has indicated its support in principle for the 20mph Speed Limit Strategy. It has consulted with bus operators about the proposals and has received no objections.

Summary

- 4.25 The key to realising substantially lower speeds on our residential roads lies in affecting a fundamental shift in attitude. The aim therefore is to build a widespread and longstanding community acceptance that 20mph is the appropriate maximum speed to travel in residential areas. Ultimately, the success or otherwise of these schemes lies primarily in the hands of the residents of this area.
- 4.26 Having considered the objections to the introduction of a 20mph speed limit in Hackenthorpe and the London Road area the officer view is that, with the exception of Sheffield Road and Beighton Road in Hackenthorpe, the reasons set out in this report for making the Speed Limit Orders outweigh the objections. It is recommended that the proposals set out in this report be approved in order to continue the delivery of the 20mph Speed Limit Strategy.

Relevant Implications

- 4.27 The cost to implement these schemes is estimated to be £71,000 for Hackenthorpe (including a part-time advisory 20mph speed limit on Beighton Road) and £43,000 for the London Road area. In addition the commuted sums for these schemes, covering future maintenance, have been estimated at £30,330 for Hackenthorpe and £22,230 for the London Road area. A total of £166,650. These schemes are to be funded by part of an overall £410k allocation of Local Transport Plan (LTP) funding for 2014/15 for 20mph works. The scheme costs will be charged to BU97985.

The total capital costs of the other schemes in the 20mph is £172k which means a total of £338k of the £410k allocation will be committed. The commuted sums for other projects in this programme have not yet been confirmed. Therefore it is possible that when the commuted sums are confirmed there will not be sufficient funding to deliver all the schemes. If this is the case, the current approvals for the citywide 20mph block will need to

be varied through the ITA. This variation will be to increase the amount available to spend on 20mph speed limit schemes in Sheffield by the required amount. Should this additional allocation not be approved, the 2014/15 the 20mph schemes will have to be prioritised and delivery delayed until sufficient funding is identified. Any agreed funding increases which lead to an overall increase in capital expenditure on the citywide 20mph block will be brought forward for authorisation through the Capital Approvals Process.

- 4.28 The Council as local highway authority have the power to vary speed limits on roads, other than trunk or restricted roads by making traffic regulation orders under section 84 of the Road Traffic Regulation Act 1984. If the Council is minded to implement 20 mph restrictions on Sheffield Road and Beighton Road, the procedure in relation to consultation and notification, which is set out in Schedule 9 of the Act and the Local Authorities' Traffic Orders (Procedure)(England and Wales) Regulations 1996 must be followed, and proper consideration given to all duly made representatives.

The Council must also have regard to the Department for Transport national policy, which encourages local authorities to consider implementing 20mph speed limits in residential areas. This policy also recognises that traffic authorities have powers to introduce 20 mph speed limits, where a school is located on a road that is not suitable for a full-time 20 mph limit, that apply only at certain times of day. In support of this, the Secretary of State has provided a special authorisation for every traffic authority to place an advisory part-time 20 mph limit sign, with flashing school warning lights.

The Council has received an objection from the Police who are not supportive of the approach the Council is taking with regard to the proposal to introduce 20 mph speed limit on Sheffield Road and Beighton Road. The Council needs to consider whether the objection outweighs the benefits of introducing the speed restriction. In reaching this decision the Council must be mindful that 20 mph speed limits are intended to be largely self-enforcing and that general compliance of a 20 mph restriction needs to be achievable without a disproportionate reliance on enforcement. The Council must also be satisfied that the proposed restriction will secure the expeditious, convenient and safe movement of vehicular and other traffic (including pedestrians). Provided the Council is so satisfied, it is acting lawfully and within its powers.

- 4.29 An Equality Impact Assessment was conducted for the September 2012 report and concluded that safer roads and reduced numbers of accidents involving traffic and pedestrians would fundamentally be positive for all local people regardless of age, sex, race, faith, disability, sexuality, etc. However, the most vulnerable members of society (i.e. the young, elderly, disabled and carers) would particularly benefit from this initiative. No negative equality impacts were identified.

5.0 ALTERNATIVE OPTIONS CONSIDERED

- 5.1 In the case of Sheffield Road and Beighton Road consideration has been given to two alternative options to that recommended in this report. The first, to introduce a 20mph limit along the full length of Sheffield Road and

Beighton Road as advertised has been discussed in paragraph 4.13 to 4.16 above. The introduction of a *mandatory* part-time 20mph speed limit in the area around the Beighton Road entrance to Rainbow Forge school has also been explored and discounted to the disproportionately high cost involved in providing the correct variable message signing required to render the limit legally enforceable.

5.2 The other objections relate to the principle of introducing sign-only 20mph speed limits into residential areas, and therefore the approved Sheffield 20mph Speed Limit Strategy. As such, no alternative options have been considered. Speeds will be monitored and the addition of further measures will be considered if appropriate, as outlined in 4.12 and 4.21 above.

6.0 REASONS FOR RECOMMENDATIONS

6.1 Reducing the speed of traffic in residential areas will, in the long term, reduce the number and severity of accidents, reduce the fear of accidents, encourage sustainable modes of travel and contribute towards the creation of a more pleasant, cohesive environment.

6.2 Having considered the objections to the introduction of a 20mph speed limit in Hackenthorpe and the London Road area the officer view is that the reasons set out in this report for making the Speed Limit Order outweigh the objections. The introduction of a 20mph speed limit in these areas would be in-keeping with the City's approved 20mph Speed Limit Strategy.

7.0 RECOMMENDATIONS

7.1 Make the Hackenthorpe and London Road area 20mph Speed Limit Orders in accordance with the Road Traffic Regulation Act 1984.

7.2 Inform the objectors accordingly.

7.3 Introduce the proposed 20mph speed limits.

7.4 Introduce an advisory part-time 20mph speed limit on parts of Beighton Road as shown in Appendix C.

Simon Green
Executive Director, Place

23 December 2014



Don't stop on the zig-zag lines

Most pupils walk to school. Of those parents who drive their children to school, most respect the safety of others and park responsibly. However, despite many appeals and warnings a few continue to stop on the 'School Keep Clear' markings at the school gates. The safety of children is too important for this to continue. In future, if you do stop on the zig-zag lines you risk a £70 fine.

What happens next?

We plan to introduce the new speed limit by the end of **March 2015**, but this will depend on the response we receive to this leaflet.

If you have any questions or comments please contact:

Simon Nelson, telephone: 0114 273 6176
email: simon.nelson@sheffield.gov.uk

If you wish to lodge an objection to the new speed limit, please put this in writing to:
 Transport, Traffic & Parking Services
 2-10 Carbrook Hall Road
 Sheffield S9 2DB

Formal objections must be received by the 17th October 2014.

This document can be supplied in alternative formats, please contact 0114 273 6176

Sheffield City Council
www.sheffield.gov.uk/20mph

20

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DP16159 / Hackenthorpe


Help us make our roads safer!




We are proposing to reduce the speed limit from **30mph to 20mph** in the Hackenthorpe area.



Proposed 20mph speed limit for Hackenthorpe



Proposed 20mph area

.....

20

Why are we doing this?

Lower speeds will help make neighbourhoods safer, more pleasant places for all, particularly our children and elderly.

- Lower speeds reduce the severity of injuries for anyone involved in a collision
- Some collisions will be avoided altogether
- People are more likely to feel safe when walking and cycling

Speed reductions in 'sign-only' 20 mph areas can be small to start with but we are committed to working with the community to spread the message that lower speeds will make the area safer for residents.

Every driver that slows down helps to make the area safer.

What will the new 20mph areas look like?

In the past, we have built road humps in 20 mph areas to keep speeds low. Those schemes have been very successful, but they are also very expensive. Cuts to the funding we receive from Central Government for transport related projects mean we can no longer afford such schemes.

New 20mph limits will be indicated by traffic signs and road markings only. This is less expensive, allowing us to reduce speeds in more residential areas. Speed limit signs will mark the entrances to each 20mph area, and small signs will be fixed to lamp posts to remind drivers of the new limit.

Regeneration and Development Services

Director: David Caulfield, RTP1
2-10 Carbrook Hall Road · Sheffield · S9 2DB
E-mail: simon.nelson@sheffield.gov.uk
Website: www.sheffield.gov.uk

Officer: Simon Nelson
Ref: SD/LT136/SN01

Tel: (0114) 2736176
Date: 09 October 2014

Dear Sir or Madam

20mph Speed Limits in Sheffield

Lower speeds reduce the number of traffic accidents and the severity of injuries. Lower vehicle speeds in our residential areas will help make neighbourhoods safer places for all residents. In common with many other cities, Sheffield City Council has adopted the principle that it is inappropriate for minor roads to be subject to the same speed limit as A- and B-roads. We believe that the maximum acceptable speed on residential roads should be 20mph and we are in the process of introducing a lower speed limit in residential areas across the city. By March 2016 approximately a third of our residential roads will be subject to a 20mph limit with more areas to be added in the coming years.

Speed reductions in sign-only 20mph areas can be small to start off with but we are committed to working with the community to spread the message that every driver that slows down helps to make our community safer.

Hackenthorpe 20mph speed limit

At the end of September we delivered a leaflet to properties in parts of Hackenthorpe explaining a proposal to introduce a 20mph speed limit in the area. Councillor Jack Scott, the Council Cabinet Member with responsibility for road safety, will consider the responses to the proposal and make a final decision about whether the scheme should go ahead later this year.

Sheffield Road and Beighton Road

The proposals for Hackenthorpe did not include altering the speed limit on Sheffield Road and Beighton Road. Your local councillors feel that this should also be included and have asked me to formally advertise the intention to introduce a 20mph limit on these roads and invite residents to comment (see the attached plan).

There are arguments for and against reducing the speed limit on Sheffield Road and Beighton Road. On the one hand it is not a purely residential road but a bus route and a main road through the area. As such some may feel that 30mph is the

appropriate limit. On the other hand lower speeds would make it easier and safer for people to cross. The main entrance to Rainbow Forge Primary School is on Beighton Road and a reduction in traffic speeds would undoubtedly be welcomed by the school community.

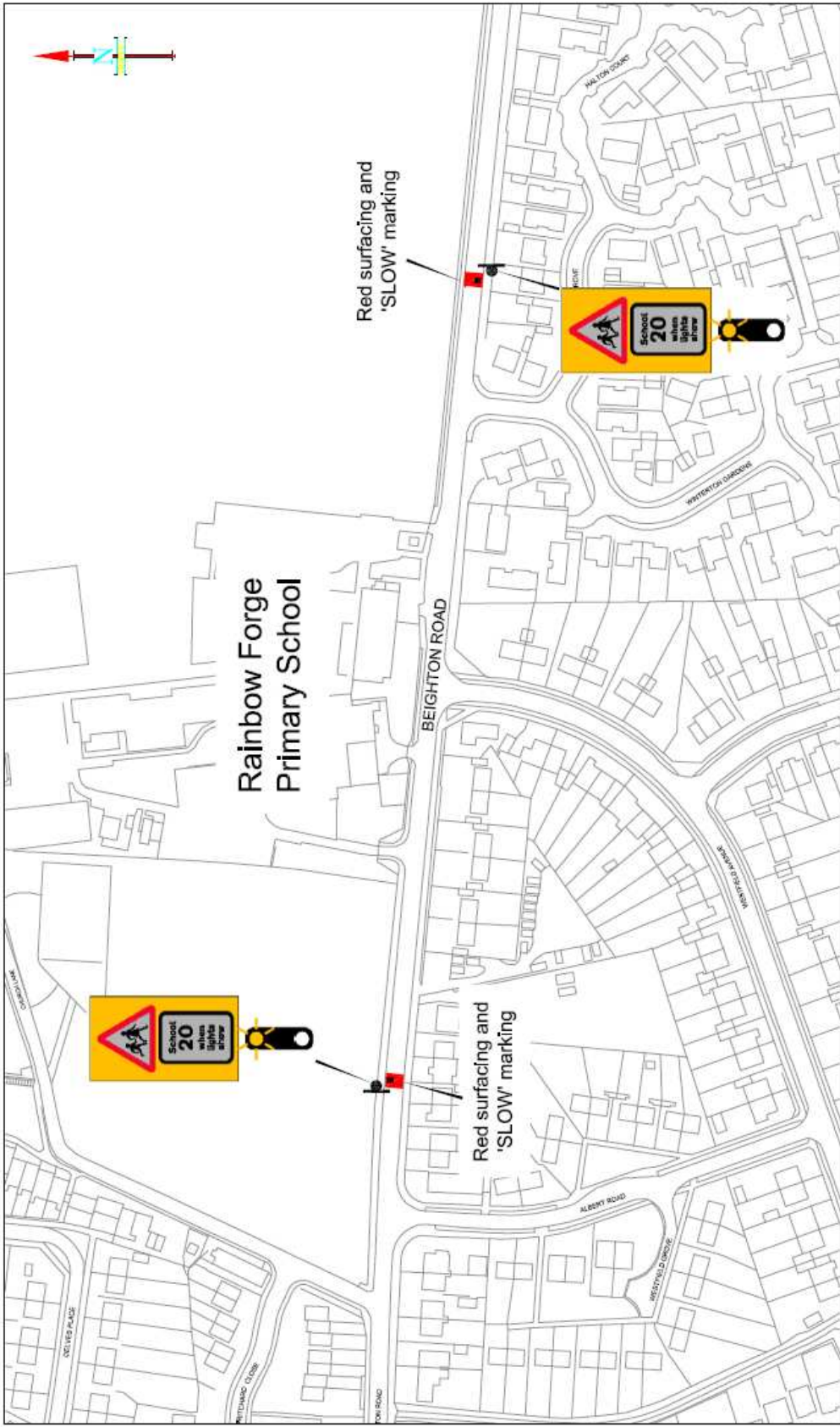
Please contact me on the above telephone number or email address if you have any questions or wish to express your support for the introduction of a 20mph speed limit on Sheffield Road and Beighton Road.

If you wish to lodge an objection you must do so in writing, either by email or by writing to me at Transport, Traffic and Parking Services, 2-10 Carbrook Hall Road, Sheffield, S9 2DB. **Any objections must be received by Friday 7 November 2014.**

Yours faithfully

A handwritten signature in black ink, appearing to read 'S. Nelson', written in a cursive style.

Simon Nelson,
Transport, Traffic & Parking Services



Hackenthorpe 20mph speed limit
Rainbow Forge Primary School advisory part-time
20mph speed limit

Not to scale

Drawn by: Sharon Allison, December 2014
 Scheme Design, Transport, Traffic & Pavement Solutions,
 Sheffield City Council

File ref: GSD/USD&P/ENG_T/BAFF/DTMScheme
 Design: T138 Hackenthorpe 20mph Advisory
 Road case-Urns advisory 20mph speed limiting

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Help us make our roads safer!



Don't stop on the zig-zag lines

Most pupils walk to school. Of those parents who drive their children to school, most respect the safety of others and park responsibly. However, despite many appeals and warnings a few continue to stop on the 'School Keep Clear' markings at the school gates. The safety of children is too important for this to continue. In future, if you do stop on the zig-zag lines you risk a £70 fine.

What happens next?

We plan to introduce the new speed limit by the end of **March 2015**, but this will depend on the response we receive to this leaflet.

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email: simon.nelson@sheffield.gov.uk

If you wish to lodge an objection to the new speed limit, please put this in writing to:
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Sheffield S9 2DB

Formal objections must be received by the 17th October 2014.

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Sheffield City Council
www.sheffield.gov.uk/20mph



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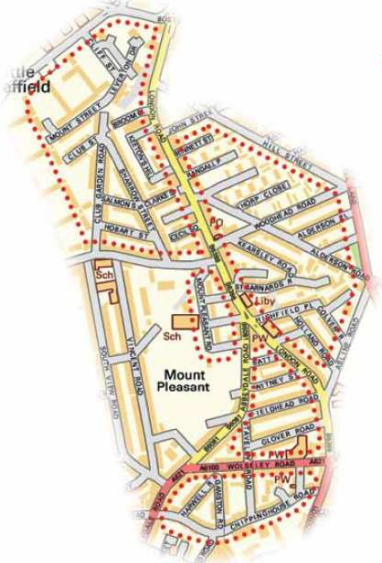
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DP16159 / London Road area



We are proposing to reduce the speed limit from **30mph** to **20mph** on the residential roads around London Road.



Proposed 20mph speed limit for the London Road area

Why are we doing this?


Lower speeds will help make neighbourhoods safer, more pleasant places for all, particularly our children and elderly.

- Lower speeds reduce the severity of injuries for anyone involved in a collision
- Some collisions will be avoided altogether
- People are more likely to feel safe when walking and cycling


Speed reductions in 'sign-only' 20 mph areas can be small to start with but we are committed to working with the community to spread the message that lower speeds will make the area safer for residents.

Every driver that slows down helps to make the area safer.

Proposed 20mph area



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What will the new 20mph areas look like?

In the past, we have built road humps in 20 mph areas to keep speeds low. Those schemes have been very successful, but they are also very expensive. Cuts to the funding we receive from Central Government for transport related projects mean we can no longer afford such schemes.

New 20mph limits will be indicated by traffic signs and road markings only. This is less expensive, allowing us to reduce speeds in more residential areas. Speed limit signs will mark the entrances to each 20mph area, and small signs will be fixed to lamp posts to remind drivers of the new limit.



SHEFFIELD CITY COUNCIL

Individual Cabinet Member Decision

Report of: Executive Director, Place

Report to: Cabinet Member for Business, Skills & Development

Date: 8th January, 2015

Subject: Objections to proposed traffic calming, Coisley Hill, Woodhouse, Accident Saving Scheme.

Author of Report: Nat Porter (ext 35031)

Key Decision: NO

Summary: The report outlines objections received to proposals for the introduction of traffic calming and a pedestrian crossing, along with associated waiting restrictions, on Coisley Hill and Sheffield Road, Woodhouse. The report seeks a decision on how the scheme should be progressed in light of these objections.

Reasons for Recommendations:

The proposed waiting restrictions are necessary to enable buses to traverse the proposed traffic calming features, which are proposed in response to recorded road traffic collisions. Failure to provide these restrictions would impede the flow of service buses and emergency services, and may result in additional noise nuisance for residents.

The proposed waiting restrictions outside Nos 155-159 Sheffield Road are proposed to ensure adequate visibility for the safety and comfort of pedestrians using the proposed crossing. The Council's Road Safety Audit Co-ordinator has indicated that retaining parking would likely be raised as an issue at the Stage 2 Road Safety Audit.

The crossing is proposed to serve the main observed pedestrian desire lines. Notwithstanding the existence of other desire lines for crossing, it is felt to be important that these are given lesser priority.

Recommendations:

- That, notwithstanding objections received, the advantages outweigh the objections and the Order be made as advertised;
 - That the proposed traffic calming, pedestrian crossing and associated works be introduced as proposed; and,
 - That those who have provided comments in response to the letter and public notice be informed of the decision accordingly.
-

Background Papers: Appendix A – Drawing of proposed scheme
Appendix B – Correspondence received and officer's comments

Category of Report: OPEN

Statutory and Council Policy Checklist

Financial Implications
YES Cleared by: D. Watkinson (11 th Dec '14)
Legal Implications
YES Cleared by: N. Wynter (1 st Dec '14)
Equality of Opportunity Implications
NO Cleared by: I. Oldershaw (28 th Nov '14)
Tackling Health Inequalities Implications
NO
Human Rights Implications
NO
Environmental and Sustainability implications
NO
Economic Impact
NO
Community Safety Implications
YES
Human Resources Implications
NO
Property Implications
NO
Area(s) Affected
Woodhouse ward
Relevant Cabinet Portfolio Lead
Cllr. Leigh Bramall
Relevant Scrutiny Committee
Economic and Environmental Wellbeing
Is the item a matter which is reserved for approval by the City Council?
NO
Press Release
NO

REPORT TO THE CABINET MEMBER FOR BUSINESS, SKILLS & DEVELOPMENT

OBJECTIONS TO PROPOSED TRAFFIC CALMING, COISLEY HILL, WOODHOUSE ACCIDENT SAVINGS SCHEME

1.0 SUMMARY

- 1.1 The report outlines objections received to proposals for the introduction of traffic calming and a pedestrian crossing, along with associated waiting restrictions, on Coisley Hill and Sheffield Road, Woodhouse.
- 1.2 The report seeks a decision on how the scheme should be progressed in light of these objections.

2.0 WHAT DOES THIS MEAN FOR SHEFFIELD PEOPLE

- 2.1 Reducing the likelihood and impact of road traffic collisions contributes to '*Safe & Secure Communities*'.
- 2.2 Ensuring pedestrians can use the highway network safely and conveniently contributes to '*A Great Place to Live*'.

3.0 OUTCOME AND SUSTAINABILITY

- Reducing the incidence of collisions resulting in injury on the highway network;
- Improving the safety and convenience of pedestrians using the highway network;
- Policy W of the Sheffield City Region Transport Strategy 2011-2026 (*To encourage safer road use and reduce casualties on our roads*); and,
- the Council's Vision For Excellent Transport In Sheffield (*a better environment; a healthier population; a safer Sheffield*);

4.0 REPORT

Background

- 4.1 Coisley Hill, Woodhouse has been identified by the Council's Transport Planning team as a site requiring remedial measures in response to recorded road traffic collisions.
- 4.2 There were 9 accidents along this route in the period 2008-2012. Two were serious. There were 4 child pedestrian casualties; these have all occurred in the evenings and not on the school journey. Out of the nine accidents, 5 occurred in the dark.

- 4.3 In response to the collision record, a scheme of road humps has been developed to moderate vehicle speeds. Additional 24 hour waiting restrictions (proposed double yellow lines) are proposed, to facilitate the movement of buses and emergency vehicles where part-width cushions are proposed.
- 4.4 The proposed traffic calming is intended to reduce the incidence of road traffic collisions, by giving drivers and others greater time to react to conflicts before they result in collision. The measures are estimated to reduce the incidence of collisions by around 25-35%.
- 4.5 The proposed scheme also includes a pedestrian crossing, to ease crossing the road for pedestrians. The crossing has been located to serve the most significant pedestrian desire line observed on site. Further waiting restrictions are proposed to maintain sightlines to the crossing.
- 4.6 24 hour no stopping restrictions are also proposed at bus stops and at the entrance to Woodhouse West Primary School. A drawing illustrating the proposals is included as Appendix A.

Consultation

- 4.7 Notices detailing the proposals were erected on-street and published in local press on 24th October, 2014. The notices invited people wishing to object to or otherwise comment on the proposals to submit their comments by 14th November, 2014.
- 4.8 In addition to this, directly affected frontagers were written to with a plan of the proposals, to draw their attention to the scheme and to invite objections and or other comments.
- 4.9 Seven responses were received in response to the notice and/or letter. These comprise –
- Two responses supporting the proposals;
 - Four responses objecting to the proposed waiting restrictions, including one with attached petition with 171 signatures; and,
 - Three responses objecting to the location of the proposed pedestrian crossing.

No responses were received objecting to the proposed road humps.

(Note that some responses objected to more than one aspect of the scheme, hence the sum of responses listed above adding to more than seven)

- 4.10 Of the responses objecting to the proposed waiting restrictions –
- One related specifically to proposed restrictions on Ashpool Fold;
 - One (the one including the petition) related specifically to waiting restrictions on Sheffield Road outside Nos 155-159;
 - One objects on the basis that there may be displacement of parking demand into Ashpool Close;

- The remaining objector raises more general concerns about the proposed restrictions.
- 4.11 The proposed restrictions on Ashpool Fold affect only the first 6 metres of the street (i.e. one car length on each side). Drivers are already advised by the Highway Code not to park within 10 metres of a road junction (rule 243). There proposed restrictions should therefore have negligible impact on both the *de jure* and *de facto* parking capacity of Ashpool Fold.
- 4.12 The restrictions proposed outside Nos 155-159 are intended to protect sightlines between drivers and pedestrians using the crossing, so as to ensure that –
- Drivers can stop in good time for pedestrians waiting at or entering the crossing; and,
 - Pedestrians can see approaching traffic and be able to determine whether or not it is safe to cross.
- 4.13 The Council's Road Safety Auditor has indicated that retaining parking at this location would in all likelihood be raised as an issue at Road Safety Audit, and that an exception report would likely to be required. Retention of parking would then have to be considered by the Head of the Transport, Traffic and Parking Service or their nominee at the arbitration stage. There would remain a risk that, notwithstanding any resolution of this committee, arbitration would not permit the retention of this parking whilst providing the zebra crossing. In any event, the time require for arbitration would be expected to delay the scheme beyond the lifetime of the funding (see paragraph 4.18).
- 4.14 The proposed restrictions may result in displacement of parking demand into side streets, and this may cause some nuisance at busy periods (such as around school times). The restrictions are, however, necessary to enable service buses to traverse the proposed speed cushions, and to protect sightlines to the proposed pedestrian crossing.
- 4.15 The objections to the location of the proposed crossing were on the basis that locating the crossing nearer to the shop or working men's club would be better, in that this would be of use to people accessing these amenities. Whilst some pedestrians were observed to cross the road in this location, numbers were observed to be far fewer than those crossing at the school, even if that demand only exists for a relatively short period of the day.
- 4.16 It is therefore judged appropriate to provide the crossing in its proposed location to ensure the greatest number of pedestrians benefit, and to ensure this desire line was not 'underprotected' relative to other, lesser desire lines. This need not preclude the introduction of additional crossing(s) in future if budget were allocated for this.
- 4.17 One of the objections to the crossing gave further grounds for objection. This is included, along with all of the other representations received and

Officer's comments, in Appendix B.

Financial implications

- 4.18 The cost to implement the proposed traffic calming is estimated at £55,000, funded from the 2014/15 LTP programme. This element of LTP funding is part of an 'Accident Savings' programme which has been approved by the Integrated Transport Authority (ITA). The scheme costs will be charged to BU92769. The commuted sum for the scheme - covering future maintenance, has been estimated at £19,250. In order to fully fund this scheme, the current approvals for the Accident Scheme Saving Block, which encompasses several other schemes, will need to be varied through the ITA as the current approval is not sufficient fund all the schemes. This variation will be to increase the amount available to spend on Accident Savings Schemes in Sheffield by around £20,000. Should this additional allocation not be approved, the 2014/15 the funding for another of the schemes in the accident savings block will have to be reduced by the amount required. In addition any agreed funding increases which lead to an overall increase in capital expenditure on the Accident Savings Block will be brought forward for authorisation through the Capital Approvals Process.
- 4.19 £70,000 has been allowed to implement the zebra crossing, 2014/15 LTP programme. This element of LTP funding is part of a 'Street Ahead enhancements' programme funded in Sheffield which has been approved by the Integrated Transport Authority (ITA). The scheme costs will be charged to BU93052. The commuted sum for the scheme - covering future maintenance, has been estimated at £17,500.

Legal implications

- 4.20 The Council in exercising its functions under the Road Traffic Regulation Act (including provision of pedestrian crossings and waiting restriction) is required under the Section 122 of the Act to (a) secure the expeditious, convenient and safe movement of traffic (including pedestrians) and (b) the provision of suitable and adequate parking facilities on and off the highway, and so far as practicable having regard to the matters listed below.

The matters to be considered before reaching any decision are:

- i) the desirability of securing and maintaining reasonable access to premises;
 - ii) the effect on the amenities of a locality and (including) the use of roads by heavy commercial vehicles;
 - iii) the national air quality strategy prepared under Section 80 of the Environment Act 1995;
 - iv) the importance of facilitating the passage of public service vehicles and of securing the safety and convenience of passengers/potential passengers; and
 - v) any other matters appearing to the Council to be relevant.
- 4.21 In response to the consultation, the Council has received a number of objections from individuals who are not supportive of the approach the

Council is taking with regard to the proposed waiting restrictions and the location of the proposed pedestrian crossing. The Council therefore needs to consider whether these objections outweigh the benefits of implementing these proposals. If the Council is satisfied that the benefits of introducing the proposals outweigh the objections, it will be acting lawfully and within its powers should it decide to implement the proposals.

4.22 ***Equality implications***

No significant equalities implications have been identified in connection with either progressing the proposed scheme, or with retaining the status quo. Any pay & display scheme would include exemptions for disabled persons' blue badge holders, from both charges and time limits.

5.0 ALTERNATIVE OPTIONS CONSIDERED

5.1 Doing minimum has been considered (i.e., not implementing the proposed traffic calming or zebra crossing). This would mean that the collision rate would be expected to continue at the observed rate, and that conditions for pedestrians crossing Coisley Hill would remain unimproved.

5.2 Reducing or removing the lengths of proposed yellow lines where the restrictions are proposed to facilitate the flow of buses over the proposed speed cushions has been considered. Failure to provide these restrictions may mean buses are unable to straddle cushions owing to parked vehicles. This would act as a hindrance to the running of service buses, and may increase the risk of incidents of passengers falling where buses are jolted by the humps. There may also be increased noises nuisance where vehicles are unable to straddle cushions.

5.3 Reducing or removing the lengths of proposed yellow lines in the vicinity of the shop at № 155 has been considered. If parking were permitted here, parked vehicles would interfere with sightlines between pedestrians approaching the crossing from the southern footway, and drivers approaching from the west. The remaining visibility would be less than that required for a motorist to come to a safe stop for a pedestrian entering the crossing at the design speed of the scheme (25mph, this being the design 'after' speed of the traffic calming); nor would it be sufficient for a pedestrian to determine that they have enough time to safely cross the road.

5.4 Replacing speed cushions with full-width road humps would allow for much of the proposed waiting restrictions to be removed, in so far as buses would be able to use the full width of the carriageway, rather than being constrained to straddling individual cushions. However, these would act as a hindrance to the running of service buses, and may increase the risk of incidents of passengers falling where buses are jolted by the humps. Full width humps could be expected to have greater implications for the emergency service than cushions. It would also not be possible to deliver a scheme with significantly revised traffic calming within the lifetime of the scheme budget.

- 5.5 Relocating the proposed pedestrian crossing to the vicinity of the Wolverley Road junction has been considered. This would address the concerns raised that the proposed crossing does not serve pedestrian movements to the shop, working men's club and bus stops. However, it would leave the stronger desire line by the school (albeit one only used for relatively short periods of the day) relatively 'underprotected', perhaps increasing risk to people crossing at this point (including the school crossing patrol). It would also not be possible to deliver a relocated crossing within the lifetime of the scheme budget.
- 5.6 Providing a crossing near to Wolverley Road in addition to the proposed crossing was considered. This would address the concerns raised that the proposed crossing does not serve pedestrian movements to the shop, working men's club and bus stops. However, the available scheme budget is not sufficient to cover the cost of two crossings.
- 5.7 Deferring the matter to allow for additional investigation and/or consultation has been considered. This would require additional time, during which the funding for the scheme would expire. This means that without a decision to build the scheme at this meeting, it would not be possible to progress a scheme.

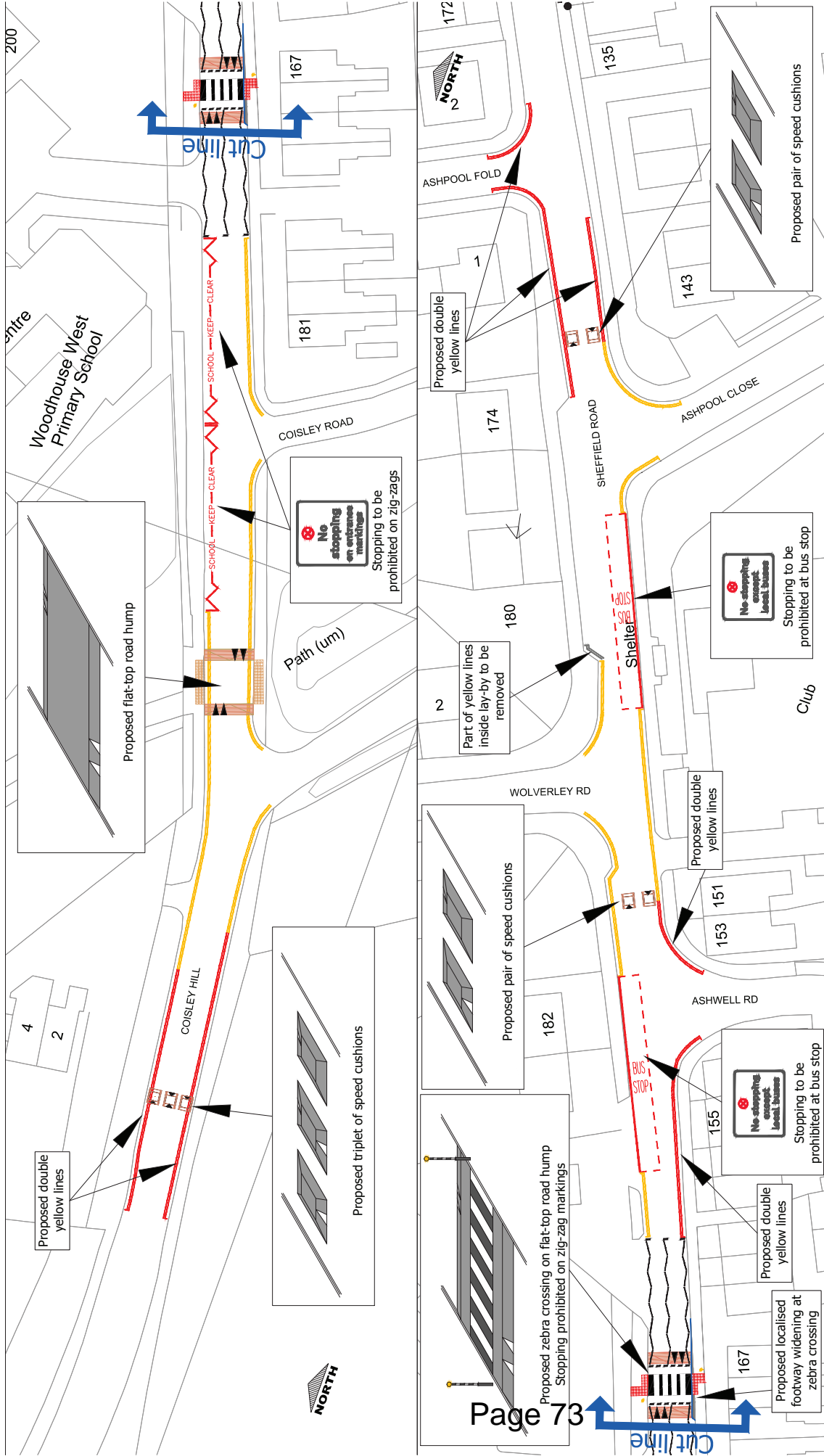
6.0 REASONS FOR RECOMMENDATIONS

- 6.1 The proposed waiting restrictions are necessary to enable buses to traverse the proposed traffic calming features, which are proposed in response to recorded road traffic collisions. Failure to provide these restrictions would impede the flow of service buses and emergency services, and may result in additional noise nuisance for residents.
- 6.2 The proposed waiting restrictions outside Nos 155-159 Sheffield Road are proposed to ensure adequate visibility for the safety and comfort of pedestrians using the proposed crossing. The Council's Road Safety Audit Co-ordinator has indicated that retaining parking would likely be raised as an issue at the Stage 2 Road Safety Audit.
- 6.2 The crossing is proposed to serve the main observed pedestrian desire lines. Notwithstanding the existence of other desire lines for crossing, it is felt to be important that these are given lesser priority

8.0 RECOMMENDATIONS

- 8.1 That, notwithstanding objections received, the advantages outweigh the objections and the Order be made as advertised;
- 8.2 That the proposed traffic calming, pedestrian crossing and associated works be introduced as proposed; and,
- 8.3 That those who have provided comments in response to the letter and public notice be informed of the decision accordingly.

Simon Green, Executive Director, Place
12th December, 2014



<p>TRANSPORT, TRAFFIC & PARKING SERVICES DIVISION SCHEME DESIGN</p> <p>2-10 CARBROOK HALL ROAD SHEFFIELD S9 2DB Tel: 0114-273-6208 Fax: 0114-273-6182 E-mail: Scheme.Design@Sheffield.gov.uk Regeneration and Development Services Director: David Gullifield, RTPI</p>		<p>Client: SHEFFIELD CITY COUNCIL</p> <p>Scheme: Coisley Hill / Sheffield Road, Woodhouses Safety & pedestrian crossing scheme</p> <p>Drawing Title: Consultation drawing</p>		<p>Drawing No. SD-0184-1590-C01</p> <p>Scale: Do not scale</p> <p>Millimetres</p> <p>Date</p>
<p>Do not scale from this drawing</p> <ul style="list-style-type: none"> Any errors/omissions to be reported immediately If in doubt, ask This drawing is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of Her Majesty's Stationery Office. © Crown Copyright. Unauthorised reproduction infringes Crown copyright and may lead to prosecution or civil proceedings. 100018816. 2014 		<p>AUTHOR: NP</p> <p>CHECKED: 25 Sep '14</p>	<p>Sheffield City Council</p> <p>SCHEME DESIGN</p>	

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APPENDIX B - Correspondence received and officer's comments

Commenter	Comment (ad verbatim)	Officer comments
A resident of Coisley Road	<p>I received your letter about the proposed traffic calming measures for Coisley Hil today.</p> <p>I would like to give my full support to these proposals. I have recently moved to Coisley Road and am horrified how fast people speed past us on the main road. Especially considering there is a primary school there. It is a nightmare crossing the road due to these speeding motorists and buses so a zebra crossing would also be most welcome.</p> <p>We have just had a baby and intend to send her to Woodhouse West Primary in a few years. I was concerned about my wife crossing that road with her twice a day and these measures would put my mind at rest.</p> <p>If I can do anything else to show my support please let me know and I will do so. Hopefully we can stop another tragedy like the girl killed up the road before it happens again.</p>	(no comments)
A resident of Ashpool Close	<p>I am a resident on Ashpool Close, just off Sheffield Road by the Woodhouse School where the proposals are located.</p> <p>I would like to welcome the new scheme, not least because of the traffic jams at school drop off and pick up times, but also because of the other problems on this road. When a bus pulls up at the stop opposite the school, this blocks the traffic, with little chance of passing safely due to the poor vision ahead because of the brow in the hill - impossible to see if anything is approaching. The long bus stops proposed should hhopefully help with this.</p> <p>The other problem is the shop at plot 154 Sheffield road - cars constantly park on this main road and block it off in the same way. The new double yellow lines here should help enormously - as long as people don't park illegally!</p> <p>I also welcome the zebra crossing. As il said earlier, the brow on the hill makes it a dangerous road to cross, so This facility will make it much safer for all, children and residents.</p>	<p>Contrary to the comments, the proposed bus stop clearways are neither intended nor expected to make it easier for drivers to be able to pass stopped buses.</p> <p>The Council has powers to issue penalty charge notices against any vehicle found to be left unlawfully on the proposed double yellow lines.</p>

Commenter	Comment (ad verbatim)	Officer comments
A resident of Ashpool Fold	<p>We have seen the notice on the end of our road regarding parking restrictions on Ashpool Fold and Sheffield Road. We both work full-time therefore we are not able to come and view the plans.</p> <p>I am guessing that due to living at the end of Ashpool Fold we will have double yellow lines outside our home.</p> <p>I would like to ask where family and friends are going to park when visiting are there going to be other allocated spaces elsewhere we can use.</p> <p>I feel for the parents of the children at the school who need to park their cars due to having to go to work after dropping their children of at school.</p> <p>I cannot understand instead of making it harder for people to park in proximity to the school could the entrance to the school be changed round so the school could be accessible from Wolverley Road or Stradbroke Road. If not why not make an overspill car park on the old recreation ground at the back of the school and make accessible via Stradbroke Road where it is not as built up area.</p>	<p>The commenter was sent a copy of the plan, enclosed with the response to their letter.</p> <p>The proposals include double yellow lines extending across part of the commenter's frontage.</p> <p>The restrictions are only proposed to apply to the southernmost 6 metres of Ashpool Fold. Although no allocated or authorised parking places are proposed the remainder of the street will remain without kerbside waiting restrictions.</p> <p>The proposed waiting restrictions are required to ensure drivers and pedestrians are able to see each other in good time at the proposed pedestrian crossing, and to ensure that buses are able to negotiate the proposed speed cushions in a manner that is comfortable and safe for passengers.</p> <p>Whilst the proposed restrictions are expected to result in greater inconvenience to parents and pupils travelling by car to and from the school, only around 25% of pupils travel by car to the school (2013 figure). This compares against the 75% of pupils who walk, who would benefit from improved safety and convenience as a consequence of the proposed crossing, traffic calming and associated waiting restrictions.</p> <p>The school's access and private parking are a matter for the school and are not of relevance to the proposed traffic scheme.</p>

Committer	Comment (ad verbatim)	Officer comments
<p>A business proprietor on Sheffield Road, with supporting petition including 171 signatures.</p>	<p>I am writing in reference to your letter dated 21st October 2014 with a reference of: 1590LTP/C1. Your letter is proposing double yellow lines in allocated areas, a zebra crossing and speed bumps on sheffield Road. I am a local shop keeper who already struggles with competitors with my field of work, so having double yellow lines will affect my trade as approximately 80% of my customers park outside to use my shop.</p> <p>I understand you are looking at the safety of the public and i am completely for that, and not against it, so the speed bumps and zebra crossing are a fantastic idea if proposed to be allocated like they are on your plans. I feel the problem you have when it comes to parking is during the school drop off and pick up times which are between 8am-9am and then 2.30pm-3.30pm. Either a no parking restriction should be put across during these hours or a time limit on parking should be added. Also, when my husband goes to buy stock, he has to pull up outside the shop to unload so again this is another issue. Deliveries are made to my store daily so this will also become a problem. Even if two car park spaces were left outside the shop for our customers then that will be really good.</p> <p>I also have a four year old son with a disability who cannot walk so this is our only access through the shop with his wheelchair so i need the parking for him aswell.</p> <p>I have been here almost 7 years and me and my husband are already struggling with our business sales and keeping up with other competitors and with your proposal of double yellow lines it could possibly close me down. I need to put food on the table for my family aswell and i feel like this could have a huge impact on us all. With this email i was going to attach a petition which i have asked my customers who feel the same, to sign, but due to technical issues with my pc i will now be sending this via post along with this letter.</p> <p>I hope you will consider my appeal fairly which i am sure you will.</p>	<p>It is acknowledged that the proposed waiting restrictions will result in the loss of about 17 metres, or about 3 car lengths, of unrestricted kerbside suitable for the leaving of vehicles, and that this would likely have a detrimental impact on customer access to the commenter's premises. Free, unrestricted parking will remain available on Sheffield Road approximately 60 metres to the east, and on side streets.</p> <p>The restrictions are proposed in the vicinity of the commenter's premises to protect sightlines between pedestrians at the zebra crossing and approaching drivers. Sightlines would need to be maintained as far as is practicable throughout the day, not just a peak traffic periods.</p> <p>The proposed restrictions will allow vehicles to wait for as long as is necessary to load or unload. Loading restrictions that would prevent this are not proposed.</p> <p>The commenter's son may be eligible of a disabled person's blue badge. This allows for a vehicle carrying the permit holder to be left on yellow lines for up to three hours.</p>

Committer	Comment (ad verbatim)	Officer comments
<p>A resident of Sheffield Road</p>	<p>I am contacting you to formally object to the proposals put forwards for traffic calming , primarily on Sheffield Road Woodhouse and not on Coisley Hill Woodhouse as stated in your documents .</p> <p>I have lived at [REDACTED] Sheffield Road Woodhouse S13 for almost 20 years and throughout those years can confirm that the speed at which traffic travels up and down is in deed dangerous and as such I agree that speed calming measures are required.</p> <p>However,I feel that the additional proposed double yellow lines will punish the residents of Sheffield Road and surrounding smaller roads for the dire way the parents dropping children at Woodhouse West School park in the mornings and afternoons. It is only at these times that parking is an issue in this area , the parents are too lazy to park in the car park at Woodhouse West End Club and as such park ANYWHERE . They have a total disregard for the Highway Code and park on double yellow lines , zig zag lines,blocking drives and on the brow on the hill . This will I fear not change simply because additional parking restrictions have been added . How do you intend to enforce the additional restrictions as there is very little policing of the current parking restrictions.at best you are simply going to push the school parking problem further away from the school gate , at worse the parents will continue to park illegally.</p> <p>With regards to the proposed zebra crossing , I feel that this is in totally the wrong position and can only be planned to benefit the school, any local residents coming from Wolverley road , the Severn side estate or above number 167 Sheffield road that need to cross the road to go to the local shop , working men's club , bus stop or to get into Woodhouse village will have to walk in the opposite direction to where they are heading to use the crossing , which will result in the crossing not been used . Currently there is a crossing lady at the school 2 times a day , but this does not stop the dozens of parents and children crossing near the bus stop to get to the shop before and after school. Surely the crossing would serve the wider community better if it was placed closer to the local amenities of the club and the shop ??? Especially as there are a lot of old and disabled people who live on ashpool close who would benefit from a crossing being closer to them .</p> <p>I would also note that coaches that take weekly swimming groups and frequent trips from school and large lorries that have to deliver to the school and the local shop will cause traffic disruption at the brow of the hill if the road has been narrowed . As a driver who has to pass this area frequently I am concerned that your proposed plans will lead to the road not been improved the crossing been used only at school opening and closing times .</p> <p>I look forward to hearing from you</p>	<p>The proposed traffic calming features are expected to reduce 85th percentile vehicle speeds from around 35mph to around 25mph.</p> <p>The proposed waiting restrictions are required to ensure drivers and pedestrians are able to see each other in good time at the proposed pedestrian crossing, and to ensure that buses are able to negotiate the proposed speed cushions in a manner that is comfortable and safe for passengers.</p> <p>The Council is able to issue penalty charges notices to motorists contravening the proposed no waiting and no stopping restrictions under the Civil Parking Enforcement régime.</p> <p>The crossing has been located on the basis of site observations, to serve busiest pedestrian flow. As the commenter alludes to, this is the flow of parents and their guardians travelling to and from the school.</p> <p>Observations indicated there was a flow of pedestrians crossing the road at other points on the street, in particular near the junction with Wolverley Road. However, these flows were observed to be very small in comparison with flows at the school gate. Therefore it was judged to provide the crossing in its proposed location to ensure the greatest number of pedestrians benefit, and to ensure this desire line was not 'underprotected' relative to other, lesser desire lines. This need not preclude the introduction of additional crossing(s) in future if budget were allocated for this.</p> <p>The localised carriageway widening is necessary to accommodate posts for beacons at the crossing whilst maintaining adequate clear footway width for the passage of pedestrians. As with the current carriageway width of approximately 6.3m, the proposed carriageway width of 6.0m would allow for two buses to pass, but would not allow two files of cars to pass a stopped bus or goods vehicle. The negative impact of the carriageway narrowing on traffic flow is therefore considered to be negligible, and would be more than outweighed by the effect of removing parking where yellow lines or zig-zag markings are proposed to maintain visibility of the proposed crossing.</p>

Commenter	Comment (ad verbatim)	Officer comments
A resident of Ashpool Close	<p>I would like to take this opportunity to oppose the planned changes to Sheffield Rd. Although I appreciate that the traffic needs slowing on this Rd I believe that the plans will not meet the needs of the local community.</p> <p>I feel that as a resident of Ashpool Close the proposal will have a significant impact on the parking on this Rd. We will be the first minor rd that traffic going to school will be able to legally park on. This will cause problems as a significant number of residents on this rd are either elderly or disabled.</p> <p>I also feel that the position of the proposed crossing is in the wrong place. I feel that as is the crossing will only be utilised for school and only at school times. I strongly feel that it would better meet the local communities needs if the crossing was placed nearer to the local shop. There are very few amenities in this locality and by placing the crossing there it can still be used by the school, but also people going to the shop and using the bus stop.</p> <p>I also feel that by placing the crossing in the planned location that this is a danger as the crossing is on the brow of an ill.</p> <p>If the rd has double yellow lines who will manage this? The Rd already has double yellow lines on a significant proportion and these are frequently ignored by parents dropping off their children at school times.</p> <p>For most of the time there is no problem with parking in that area as local residents do not park there car there for any significant amount of time.</p> <p>As an employee for a disabled client that lives on Sheffield Rd I oppose the plan as I collect him from his home at the time that parents are dropping of their children for school. As the plans are I will have to pull on to the crossing at its busiest time in order to pull on to my clients drive. I believe this to be a very dangerous situation and again cannot understand why the crossing can not be placed closer to the shop and bus stop alleviating this situation.</p> <p>I agree that a crossing and rd calming measure are needed in this area but disagree with then planned location. It appears that this is aimed towards those attending the school only as most other local residents will not use this crossing. It really should be located much closer to ashpool close so that the disabled residents in that location can access this.</p> <p>I look forward to hearing from you soon.</p>	<p>Whilst there may be a displacement of parking demand owing to the proposed waiting restrictions, it is considered that this would be dissipated across side streets. Coisley Road, Ashwell Road and Wolverley Road are all closer to the school than Ashpool Close, and it is therefore considered that any displacement into Ashpool Close would likely be relatively minor.</p> <p>Observations indicated there was a flow of pedestrians crossing the road at other points on the street away from the school, in particular near the junction with Wolverley Road. However, these flows were observed to be very small in comparison with flows at the school gate. Therefore it was judged to provide the crossing in its proposed location to ensure the greatest number of pedestrians benefit, and to ensure this desire line was not 'underprotected' relative to other, lesser desire lines. This need not preclude the introduction of additional crossing(s) in future if budget were allocated for this.</p> <p>The proposal has been subject to a Stage 1 Road Safety Audit. This auditor did not identify an issue with the proposed location of the crossing.</p> <p>The Council has powers to enforce vehicles left unlawfully on double yellow lines, under the civil parking enforcement régime. It is permissible to wait on double yellow lines for as long as is necessary to pick up or set down passengers.</p> <p>Whilst the commenter may feel there is no issue with parking at present, the proposed restrictions are necessary to enable buses to traverse the proposed speed cushions in a manner that is safe and comfortable for passengers, as well to protect sightlines at the proposed zebra crossing.</p> <p>The proximity of the crossing to driveways is acknowledged to present a risk of conflict between users of the crossing and drivers manoeuvring in to or out of accesses</p> <p>It was judged to provide the crossing in its proposed location to ensure the greatest number of pedestrians benefit, and to ensure this desire line was not 'underprotected' relative to other, lesser desire lines. This need not preclude the introduction of additional crossing(s) in future if budget were allocated for this.</p>

Committer	Comment (ad verbatim)	Officer comments
A resident of Sheffield Road	<p>(1) I do agree that traffic does need slowing down. I object to the crossing position.</p> <p>(2) Zig-zag lines and yellow lines outside school does not stop people parking. Biggest culprits for parking on yellow lines during school times being the parents who start to arrive up to at least ½hr before school finished, blocking all side roads with or without yellow lines also on Sheffield Road and Coisley Hill. School closes gates so they can't park on the drive.</p> <p>(3) Coaches for school park on brow of hill and on zig-zag lines you can't see to cross road traffic can't see you, Lorries at times also park on zig-zag lines to deliver to school.</p> <p>(4) Position of crossing means this is only going to be used mainly at school times 15-20 mins in the morning and 15-20 mins when school is finished during term time which a crossing warden is provided.</p> <p>(5) Residents will have flashing lights for 52 weeks a year 24 hours a day.</p> <p>(6) Crossings to my understanding is you have to have got your foot on crossing for drivers to stop for you. The crossing on Tannary Street you have to take your life in your hands as drivers will not stop for you.</p> <p>(7) Both parents with children and children on their own cross the road near bus stop and the shop, they are not going to walk down to crossing and back up to the shop.</p> <p>(8) Crossing is going to be between two drives mine and school drive and that the footpath is going to be widened. There is no mention of how you intend to do this, no measurements and i am worried that it could make it more difficult to get on/off my drive. Both my brother and myself are disabled and need to have good access and do not know how this will effect this. Lorries delivering outside of the school have to reverse outside my house blocking road and footpath to get up school drive which could be dangerous on the crossing.</p> <p>(9) I have lived on Sheffield Road for almost 48 yrs and before that on Southsea Road for 15 yrs. Accidents involving pedestrians have been approx. between shop and west end club.</p> <p>(10) Residents need proper consultation to put their concerns, not only from school and parents who attend the school who cause a lot of the problems at school times.</p>	<p>The existing 'school keep clear' markings are advisory. The proposed scheme will make these mandatory, which would allow the Council's Parking Services team to issue Penalty Charges notices to vehicles left in contravention of the restrictions.</p> <p>This is acknowledged. Notwithstanding this, it was judged to provide the crossing in its proposed location to ensure the greatest number of pedestrians benefit, and to ensure this desire line was not 'underprotected' relative to other, lesser desire lines. This need not preclude the introduction of additional crossing(s) in future if budget were allocated for this.</p> <p>Bellisha beacons are to incorporate cowls to minimise light trespass. It is the case that drivers are not legally obliged to stop until a pedestrian has entered the crossing; however, drivers are expected to slow down or stop to let pedestrians cross (Highway Code rule 195).</p> <p>It is neither intended nor expected that the crossing will serve all pedestrian crossing movements across Sheffield Road.</p> <p>The proposed footway widening is minor and will leave 6m clear carriageway width. This is not anticipated to adversely impact upon manœuvres to or from the objector's driveway.</p> <p>The risk around vehicles manœuvring when gaining access to the school is acknowledged; it is however noted that the school closes their access during school starting and finishing times; these being the times during which the crossing is expected to see most of its use.</p> <p>Police records of collisions indicate collisions are distributed throughout Sheffield Road between Coisley Road and Ashpool Road. The proposed traffic calming is intended to reduce the incidence and impact of collisions. The crossing, whilst expected to make it easier to cross the road, is not expected to result in fewer collisions in itself.</p> <p>The scheme has been developed on the basis of collision records and site observations. It has not been developed specifically in response to concerns raised by the school.</p>

Committer	Comment (ad verbatim)	Officer comments
A resident of Sheffield Road (continued)	<p>(11) If all propose changes were to happen who is going to police them as they will be ignored as they are now.</p> <p>(12) Hope to hear from you in the very near future regards consultation with residents.</p>	<p>The proposed waiting and stopping restrictions can be enforced by the Council under the civil parking enforcement régime.</p> <p>Owing to time and funding constraints, it is not proposed to consult with residents further on this scheme.</p>

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SHEFFIELD CITY COUNCIL

Individual Cabinet Member Report

Report of: Countryside and Environment

Report to: Cabinet Member for Environment, Re-cycling and Streetscene

Date: 8th January 2015

Subject: Cat Lane / Carfield Lane – Proposed Prohibition of Driving Order

Author of Report: Dave Aspinall (Woodlands Manager)

Key Decision: NO

Summary: Regular issues with large scale fly-tipping on and either side of Cat Lane and PROW – SHE\336.

Traffic regulation order applied for Proposed Prohibition of Driving Order - Cat Lane / Carfield Lane.

One objection received.

All points answered, but objection not retracted.

Reasons for Recommendations:

Broad political and local support for taking action to restrict the ability of fly-tippers to move around this area.

One objection received, to which all points have been reasonably taken into consideration and answered in writing.

Recommendations:

Progress the traffic regulation order.

Background Papers:

turner - cat lane carfield lane objection.pdf

Turner cat lane objection reply.doc

Appendix 1 – Map showing location of the Lane and Gate

Category of Report: OPEN

Statutory and Council Policy Checklist

Financial Implications
YES Cleared by: Janine Scarborough
Legal Implications
YES Cleared by: Paul Bellingham
Equality of Opportunity Implications
NO Cleared by:
Tackling Health Inequalities Implications
NO
Human Rights Implications
NO
Environmental and Sustainability implications
YES
Economic Impact
NO
Community Safety Implications
YES
Human Resources Implications
NO
Property Implications
NO
Area(s) Affected
Gleadless Valley Ward
Relevant Cabinet Portfolio Lead
Cllr Jane Dunn
Relevant Scrutiny Committee
Is the item a matter which is reserved for approval by the City Council?
YES
Press Release
NO

Report to the Cabinet Member for Environment, Recycling and Streetscene

Cat Lane / Carfield Lane – Proposed Prohibition of Driving Order

1.0 SUMMARY

- 1.1 Sheffield City Council (SCC) want to restrict vehicular access to Cat Lane to help reduce incidences of fly-tipping and burnt out vehicles, which are currently common at this location.
- 1.2 A Traffic Regulation Order (TRO) is the legal process allowing the use of a road, or any part of it, by vehicular traffic to be prohibited. Where a TRO has been made in such circumstances the Council are permitted to place, at or near that point, bollards, or other obstructions as they consider appropriate to prevent the passage of vehicles.
- 1.3 It is proposed that a gate shall be closed at this location but this is currently stalled due to one objection to the TRO, to which all points have been answered in writing.

2.0 WHAT DOES THIS MEAN FOR SHEFFIELD PEOPLE

- 2.1 Members of the general public will no longer have the right to take motorised vehicles along a section of Cat Lane and Carfield Lane as per attached map attached as Appendix A.
- 2.2 The preventative fly tipping measures (gate) being put in place affect one residence of Rose Cottage and this resident will be permitted rights of access to their property via a set of keys. The emergency services are also supplied keys and will be equipped to cut the lock in place if required.

3.0 OUTCOME AND SUSTAINABILITY

- 3.1 Reduced instances of Anti-social behaviour (ASB) including burnt out vehicles on the Public Right Of Way (PROW) track and fly-tipping on the PROW and either side of it on The Countryside and Environment teams land.
- 3.2 Funding has been secured for the Traffic Regulation Order and the gate is already in place (locked open) ready for this being progressed. This gate will block the through route from Cat Lane via Carfield Lane, which is that preferred by fly tippers in particular. It is broadly believed that just one gate will have the desired effect of reducing fly-tipping at this location. The site will be monitored for instances of fly tipping and burnt out vehicles after this intervention to ascertain if further action is required. This was the actions agreed at a site visit between local members, local interest groups and some local residents in 2013.

4.0 Background

- 4.1 The Friends of Cat Lane Woods contacted Gleadless Valley Ward

members in September 2013, complaining of large scale fly-tipping on and around Cat Lane, both on the highway and Public Right of Way sections and on the surrounding wood land. Shortly after this contact was made, there was a site meeting, involving several members of The Friends of Cat Lane Woods, representatives from The Countryside and Environment team and an elected Member. At which gating options were discussed and the best course of action was agreed that the best course of action would be to put a prohibition of traffic regulation in place to restrict vehicular access along a section of Cat Lane and Carfield Lane. With 1 gate in place on Cat Lane to stop there being a through route for fly-tippers in the hope that this alone rectifies the problem. If it does not, there is scope within this traffic regulation order (TRO) to put a further gate on Carfield Lane to stop all vehicular access into this area. The TRO and gate has been funded by Gleadless Valley Ward members and the gate is physically in place awaiting the formal TRO to be processed. The gate has been fitted by the Countryside and Environment team.

4.2 The Countryside and Environment team concur that this is a significant problem for them to clean on a regular bases. The PROW team, state that there have been issues with burnt out vehicles in the same location.

4.3 One objection has been made. With four major points:

- Access for the emergency services.
- Leaving the resident of Rose Cottage stranded.
- SCC using the TRO as an excuse to no longer properly maintain the public rights of way and woodlands in within this area.
- Fly-tipping will not be irradiated by only one gate, as there will still be access to the area.

Three points raised have been answered, in writing:

- Use of such gates is common practice city wide, the fire brigade are furnished with keys, but in practice will cut their way in, in emergencies.
- The resident of Rose Cottage is aware and their access will be maintained.
- The Countryside and Environment team complete annual maintenance in the area and the gate will not change this, indeed the standards in the area were subsequently checked on the back of the letter received as part of the objection.

The fourth point has been covered from the outset as it is broadly agreed that two gates would be overkill in the first instance and that most fly-tippers prefer a through route and so will no longer fly-tip due to this one gate.

But the objection has not been removed.

5.0 ALTERNATIVE OPTIONS CONSIDERED

5.1 Apply for TRO and fit 2 gates to completely restrict vehicular access from the outset. It was preference of all in attendance at the meeting to have a TRO that allows for this, but to only put one physical gate in place in the first instance, to ease access to Rose Cottage, whilst restricting the through route that fly-tippers currently enjoy. This was discussed and agreed as the best course of action at the site visit in September 2013.

5.2 Monitor the area and continue to remove fly-tipping.

Fly-tipping is removed from the public right of way itself by Amey under the streets ahead contract.

Fly-tipping is removed from the adjacent lands by the Countryside and Environment team.

This is not sustainable for Countryside and Environment team in particular who struggle to keep on top of the issue in this area. The local public including The Friends of Cat Lane Woods are calling for more robust and sustained action by Sheffield City Council.

6.0 REASONS FOR RECOMMENDATIONS

6.1 Fly tipping is a problem in this area and current measures are not sufficient for the local users of the area. This TRO and gate will remove the through route, which are known to be preferred by fly-tippers, therefore reducing the incidences of fly-tipping at this location and increasing the amenity of this area for the local users of the area.

6.2 Agreement from PROW, The Countryside and Environment team , Highways Maintenance including Amey, local members and The Friends of Cat Lane Woods that this is the best course of action.

6.3 Whilst the issues raised by the objector are noted it is felt that these issues have all been considered and addressed and that the benefits of proceeding with the TRO outweigh the outstanding objection.

7.0 LEGAL IMPLICATIONS

7.1 The Council has power to make a Traffic Regulation Order under Section 1 of the Road Traffic Regulation Act 1984 ("the 1984 Act") including those having the effect of prohibiting the use of a road, or any part of it. In exercising that power it shall be the duty of the Council, in accordance with Section 122 of the 1984 Act, and so far as is practicable and whilst having regard to the matters set out in subsection (2), to secure the expeditious, convenient and safe movement of vehicular and other traffic (including pedestrians).

7.2 Before the Council can make a TRO it must follow prescribed procedures in accordance with the Local Authorities' Traffic Orders (Procedure)

(England and Wales) Regulations 1996. This includes publishing notice of its intention in a local newspaper and allowing any person to object to the proposals. Where objections are received Regulation 13 places a duty on the Council to ensure that these objections are considered.

7.3 So long as the Council are satisfied that the requirements set out in the previous two paragraphs are satisfied then it is acting lawfully and within its powers.

7.4 Section 92 of the 1984 Act permits the Council, where the passage, or the passage in any direction, of vehicles is prohibited at any point of a road by an order made under Section 1 of the Act to place, or authorise or require to be placed, at or near that point such bollards or other obstructions as they consider appropriate for preventing their passage.

8.0 FINANCIAL IMPLICATIONS

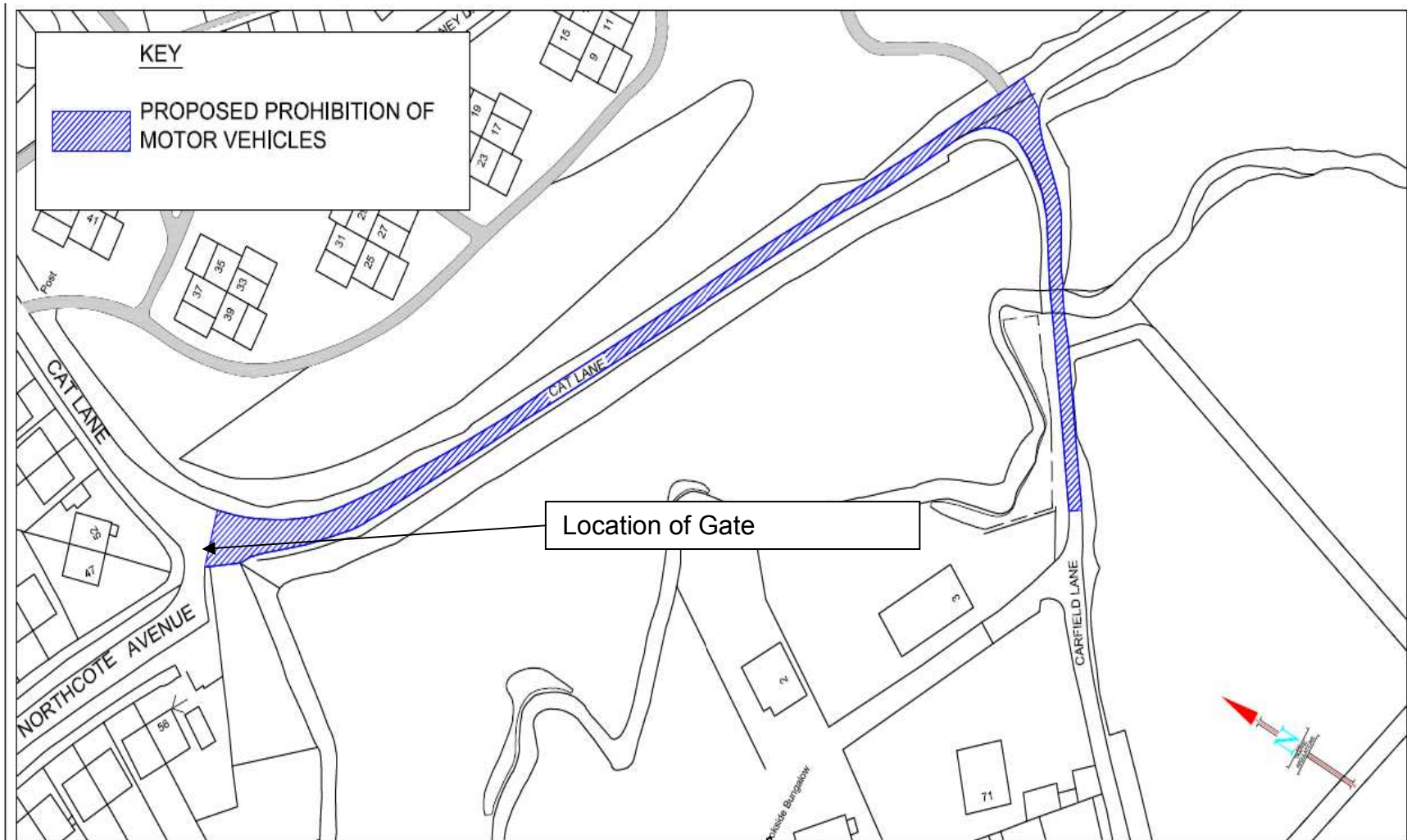
8.1 The cost of producing and processing the order and the first gate were £4,500 and £1000 respectively. Funding was secured from the Gleadless Valley Ward pot reflecting the support from ward councillors. It currently costs up to £6000/ annum to clear the litter and fly-tipping from the Lane which would be a saving to the Council.

9.0 RECOMMENDATIONS

9.1 Having considered the objection(s) to the proposed Traffic Regulation Order it is recommended for the reasons set out in this report that :-

- (i) The objection should be overruled,
- (ii) The Traffic Regulation Order should be made in accordance with the Road Traffic Regulation Act 1984
- (iii) The objector be informed accordingly, and
- (iv) The necessary work to implement the closure of Cat Lane be carried out
- (v) Authorise the gate to be locked shut to prevent the passage of vehicles.

Author Dave Aspinall (Woodlands Manager)
Department Culture and Environment
Date 13th November 2014



DEVELOPMENT SERVICES TRANSPORT & HIGHWAYS DIVISION TRAFFIC SECTION 2-10 CARBROOK HALL ROAD SHEFFIELD S9 2DB E-mail: Traffic.Reg@Sheffield.gov.uk Regeneration and Development Services Director: David Caulfield, RTPI	<ul style="list-style-type: none"> Do not scale from this drawing Any errors/omissions to be reported immediately If in doubt, ask © Crown copyright. All rights reserved. 10018816, 2010 	Client	SHEFFIELD CITY COUNCIL		Drawing No.	TR/36/01/ENV1/CL	
		Scheme	CAT LANE		Scale	NTS	
A Service Area of the Directorate Place Sheffield City Council	DRAWN NC APRIL 2014	CHECKED	Drawing Title PROPOSED PROHIBITION OF DRIVING Traffic Regulations Drawing		0 30 Millimetres 	Date APRIL 2014	TRAFFIC REGULATIONS

358686-10/7/14.

9th July 2014
Dear Sir,

I wish to object to the proposed order regarding the prohibition on driving on parts of Cat Lane / Carfield Lane. We have been advised the closure of the entrance on Cat Lane is partially to prevent fly tipping.

I wish to object to the closure of the Cat Lane entrance on the grounds of public health and safety due to the access from Carfield Lane not being of sufficient width to accommodate some emergency vehicles that would have to enter the grounds.

According to the fire safety department access for appliances and firefighting facilities for fire and rescue service use, the minimum width of an access road for pumping appliances is 3.7 m and for hydraulic platforms 3.7m. The minimum width of gateways is 3.1 m and the bridge weights must exceed the laden weight of 17 tonnes for a hydraulic platform.

(Please see attached photo of the above document)

The access from Carfield Lane, which has not been gated is only 2.9m and is normally restricted by vehicles. Where the wall begins halfway down the road narrows to only 2.6m wide. The bridge has had work carried out and walls erected either side, these are within the minimum width required according to the fire safety document. Unfortunately the banking before the bridge and roadway only measures 2.8m wide while the point just after the bridge measures 2.9m wide to the edge of the drop.

The entrance on Cat Lane that has been proposed to be closed and has already had a gate established exceeds these measurements for vehicles to gain access with no turns to accommodate.

If fly tipping is a problem then surely it would make sense to gate both entrances to prevent vehicle access, not just one!

I believe that it would make more sense to leave the wider roadway and more direct route off Cat Lane open for access to both the wooded area and Rose Cottage. I believe this would adhere to the public health and safety with regards to the emergency vehicle access requirements.

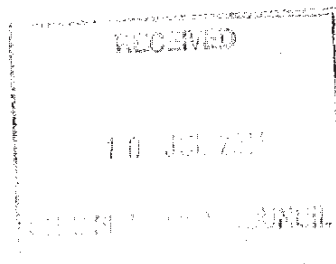
The Cat Lane entrance would in my view be the easier entrance point for any emergency vehicle that would need entry as it is straighter than the proposed route from Carfield Lane and does not get obstructed by other vehicles parking at the entrance, therefore saving valuable seconds which can all too easily be a matter of life or death.

If the proposal is to be granted and go ahead will the grounds still be maintained? Since the prohibition proposal has been displayed on the gate, I have noticed that there has been no maintenance of the roadway and it is only due to vehicles travelling through that has kept the over growing shrubbery at bay. Further along where in previous years the Carwood entrance has been gated and locked, the plant life has gone completely wild and where there were once tracks leading in all different directions, you can no longer make them out. There is a narrow path where if you are lucky two people can walk side by side, shoulders touching, before it narrows again and you have to weave through brambles. One path which was popular with dog walkers is now inaccessible. My fear is that if this is gated this place will become overgrown, more than it already is and become disused, leaving the resident of Rose Cottage stranded. Although this resident at the present time does not require the use of a vehicle, future residents may rely on them. Has any of this been taken into account?

I wish to be personally informed by way of letter, the outcome of the investigation which I hope my objection should instigate and the resulting outcome of the prohibition proposal.

Yours faithfully


Mr M. Turner



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Regeneration and Development Services

Director: David Caulfield, RTP1

Traffic Section: 2-10 Carbrook Hall Road, Sheffield, S9 2DB

E:mail: nel.corker@sheffield.gov.uk Fax No. (0114) 273 6182

Website: www.sheffield.gov.uk

Officer: Nel Corker

Tel: (0114) 273 6086

Our Ref: TR/20/1009

Date: 17 July 2014

Dear Mr M Turner

Cat Lane/Carfield Lane – Proposed Prohibition of Driving Order

Thank you for your recent correspondence regarding the above scheme.

In response to your query, the proposed Prohibition of Driving Order on parts of Cat Lane and Carfield Lane has an exemption for police, fire brigade or ambulance purposes in an emergency. The access for the Fire Service would not change. There are a number of metal barriers across the city, which the Fire Service are aware of and have keys for, however, in the case of an emergency they have no problems gaining access as they simply cut their way in.

I have contacted my colleagues in the Parks and Countryside Department regarding maintenance of the grounds. They have responded to say that pot holes have been filled in, dangerous trees dealt with and fly-tipping removed. They cut the vegetation back once a year but will take note of your letter and check the current status.

The resident of Rose Cottage is aware of the proposals and their access will be maintained.

I hope that this has helped answer your queries. Please inform me if you still wish for your objection to the proposals to remain by the 1st August 2014. If you have any further queries please do not hesitate to contact me and I will try and answer them.

Yours sincerely,

Nel Corker
Traffic Regulations Group
Transport, Traffic and Parking Services

Large print versions of this letter are available by
telephoning (0114) 273 6157 Page 93

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